

Project Description and Design Basis

The Cui Family Land Subdivision in Point Reyes Station

Marin County, California

Project ID P4403

Vesting Tentative Map Application

January 2025

Background: *California has a housing supply and affordability crisis of historic proportions. The crisis has grown so acute in California that supply, demand, and affordability fundamentals are characterized in the negative conditions: underserved demands, constrained supply, and protracted unaffordability. California's overall home ownership rate is at its lowest level since the 1940s. The state ranks 49th out of the 50 states in home ownership rates as well as in the supply of housing per capita. Lack of supply and rising costs are compounding inequality and limiting advancement opportunities for many Californians. To address this crisis, the Legislature has enacted numerous statutes intended to significantly increase the approval, development, and affordability of housing for all income levels.*

This Project would provide support in housing development in Marin County to comply with the state housing mandate.

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1.0 Project Location and Overview

The subject property (82.32 acres) is located near 11798 State Route 1, Point Reyes Station in the unincorporated area of Marin County (APN: 119-050-04, 119-050-09, 119-140-03, and 119-140-09). It is within the village limit boundaries of the community of Point Reyes Station and located within the Coastal Zone. Figure 1a shows the property location in Marin County and Figure 1b shows the project vicinity and the existing property parcels (4 parcels). The property is bounded by Point Reyes-Petaluma Road and Lagunitas Creek on the south, State Route 1 to the west, rural residential housing to the north, and open pastureland to the east. The project site is within a Coastal Commission's Categorical Exclusion Area and is currently vacant, except for an equestrian facility lease.

The County GIS Database provides the following information:

- The Project site is located within an Urban Wildland Interface;
- The Project site is not located within the Ridge and Upland Greenbelt (RUG);
- The Project site includes small portions of high archaeological sensitivity;
- The Project site is in a moderate fire hazard severity zone.

Figure 2a shows the Coastal Zone land use zoning and the property location. The property is within the zoning of C-ARP-3 -- Coastal, Agricultural, Residential Planned (1 unit per 3 acres). The C-ARP land use zoning is designed to preserve productive lands for agricultural use through the clustering of allowed residential development.

The project site is subject to the Marin Countywide Plan (CWP)'s C-AG3 (Coastal Agricultural 3; 1 unit per 1 – 9 acres) land use designation. The C-AG3 land use category was established for residential use within the context of small-scale agricultural and agriculturally related uses. According to Map 19e Point Reyes Station Land Use Policy Map, the density associated with C-AG3 is 1 unit per every 1 to 9 acres (see Figure 2b).

The 82.32-acre subject property is proposed to be subdivided into 37 parcels for future single-family residential development (see Figure 3). Of the 37 parcels, 5 parcels are designated as affordable housing. The subdivision considered a density bonus of 35% after meeting the 20% affordable housing requirement¹. Preliminary plan shown in Figure 3 indicates that individual lots will range between about 1.02- and 8.2-acres, meeting the density requirement for the C-AG3 land use designation (1 unit per 1 to 9 acres).

The land subdivision proposal includes conceptual plans for new driveways to access all the new parcels and driveway tie-in aprons at State Route 1 and Point Reyes-Petaluma Road, roadway improvements to Point Reyes-Petaluma Road at the southeast corner, utility installation and maintenance areas, onsite wastewater treatment systems, stormwater drainage systems, limited tree removal, and general grading to accommodate site improvements such as new driveways.

¹ Number of base units: $82.32 \div 3 = 27.44$ units;

Required number of affordable housing units (20%): $27.44 \times 20\% = 5.49$ units -- 5 units for affordable housing and 0.49 unit for an in-lieu fee;

Total number of units with 35% density bonus: $27.44 \times (1 + 35\%) = 37$ units.

Access to the development will be provided via an eastward extension of the existing Water Tank Road, via a new access driveway from State Route 1 a few hundred yards north of the existing Water Tank Road, and a new driveway extending from Point Reyes-Petaluma Road in the southeast corner of the site (see Figure 3).

2.0 Project Team

The Project team consists of highly respected local firms. These include sanitary engineers, civil engineers, hydrologists, biologists, archaeologists, traffic analysts, arborists, geotechnical engineers, and surveyors. Table 1 below is a summary of the Project team.

Table 1 Project Team

Firm	Subject Area	Contact
NexGen Engineering & Consulting LLC	<ul style="list-style-type: none"> Design Plans (Site Plan, Grading Plan, Constraints Map, OWTS and Drainage Plans, Utilities Plan, Tentative Map) Septic System Analysis and Design 	Ian Cole, P.E., President 1043 Nichols Dr Suite 200 Rocklin, CA 95765 Email: ian@nexgeneng.com Tel: (916) 413-7238
Stetson Engineers Inc.	<ul style="list-style-type: none"> Drainage Study Stormwater Control Plan Traffic and Circulation Study Water Supply System Design and Management 	Xiaoqing Zeng, PhD, PE, Special Project Director/ Supervising Engineer 2171 E. Francisco Blvd., Suite K, San Rafael, CA 94901 Email: xiaoqingz@stetsonengineers.com Tel: (415)457-0701
Huffman-Broadway Group, Inc.	Biological Site Assessment	Robert F. Perrera, Senior Wetland Scientist. 1101 5th Avenue, Suite 205 San Rafael, CA 94901-3358 Email: rperrera@h-bgroup.com Tel: (415)385-4106
Evans & DeShazo Inc.	Culture/Archaeological Resources Study	Sally Evans, M.A., RPA. 1141 Gravenstein Hwy S Sebastopol, CA 95472 Email: sally@evans-deshazo.com Tel: (707)823-7400
Urban Forestry Associates, Inc.	Arborist's Report	Ben Anderson 157 N McDowell Blvd, PMB 101 Petaluma, CA 94954 Email: ben@urbanforestryassociates.com Tel: (805) 748 3124
Cinquini & Passarino, Inc.	Tree Survey	James Kulpa, C.H. 1360 North Dutton Avenue, Suite 150 Santa Rosa, California 95401 Email: jkulpa@cinquinipassarino.com Tel: (707) 542-6268
Miller Pacific Engineering Group	Geotechnical Feasibility Evaluation	Nathan Klemin, P.E., G.E, Senior Engineer 1360 Redwood Way, Suite B Petaluma, CA 94954 Email: NKlemin@millerpac.com Tel: (707)765-6140
Adobe Associates Inc.	Boundary and Topographic Survey	Ray Carlson, PLS 1220 N Dutton Ave, Santa Rosa, CA 95401

3.0 Project Objectives

- Implement the goals, objectives, and policies set forth in the Marin County Local Coastal Program (LCP) land use plan (LUP), the Marin Countywide Plan (CWP), and the Point Reyes Station Community Plan;
- Subdivide the land in accordance with the LCP's land use and zoning density requirements;
- Implement the goals of the Coastal Commission's Categorical Exclusion Areas.
- Create a comfortable, quiet family agricultural community of all single-family homes;
- Set aside a portion of lot units for affordable housing in accordance with the requirements of Marin County Municipal Code, Title 20, and plan to construct affordable housing units in higher priority than the market-rate housing units;
- Ensure that the project is economically viable and no loss so that it becomes a sustainable project;
- Preserve the Project site's environmentally sensitive resources;
- Provide for a neighborhood public space;
- Incorporate sustainable design elements and the latest conservation technologies, consistent with creating a distinct family agricultural community;
- Build a family-based, micro-organic farm to raise fruits, vegetables, flowers, nuts and other crops.

4.0 Planning Context

The land subdivision will be developed in the context of local and regional plans, policies, and programs governing the design and implementation of the proposed community.

Marin Countywide Plan (CWP)

The Marin CWP is the governing general plan for the unincorporated areas of the County and establishes goals, policies, and programs that govern existing and future land uses and developments. The CWP also includes adopted community area plans as they pertain to specific unincorporated communities. The property is within the Coastal Corridor, which is primarily designated for open space, recreational uses, agriculture, and the preservation of existing coastal communities.

The CWP contains numerous environmental quality and community development policies which will apply to future development of the project site. The CWP includes policies that are intended to protect existing vegetation and animal habitats, maintain unique geological, ecological, and archaeological sites, and preserve native trees and oak woodland habitat, streams and riparian systems, and shorelines. The policies encourage development to be designed and sited to preserve agriculture and protect natural resources. In addition, community development policies specify that new structures should be designed to avoid environmental constraints, such as wetlands and streams.

This subdivision plan is prepared to be consistent with the Marin Countywide Plan (CWP). The implementation of the CWP's goals and policies runs through the entire design process.

Marin County Local Coastal Program (LCP)

The Marin County LCP is comprised of a land use plan and Coastal Zoning Code for the County's coastal areas. The LCP was mandated by the California Coastal Act of 1976 which established a statewide coastal management program to ensure that coastal resources are properly utilized and protected. The Marin County LCP-Land Use Plan (LUP) contains policies that are intended to protect and promote public access to the County's coastal areas, promote and encourage recreation and visitor serving facilities, protect, maintain, and promote appropriate development and access to federal parklands, protect resources including streams and riparian habitats, wetlands and coastal dunes and agricultural lands, and ensure adequate and appropriate public services as well as land use and new development.

The LCP consists of the following two parts:

- The Land Use Plan (LUP), which sets forth broad goals and policies for the coastal zone and its unique communities.
- The Coastal Zoning Code (Title 20), also known as the Implementation Plan, provides more detailed requirements and standards for development in the coastal zone.

The primary tool for implementing the LCP is the "Coastal Development Permit". However, certain types of development in certain areas of the Coastal Zone could be excluded from the Coastal Development Permit requirement under the Categorical Exclusion Orders. This Project site is located within the area of Categorical Exclusion.

The Coastal Zoning Code provides specific standards of implementing the policies and programs of the LCP Land Use Plan. The main provisions related to this land subdivision design include:

- 20.65.050 - C-ARP Zoning District Standards.
- 20.65.030 - Planned District General Development Standards.
- Chapter 20.64 - Coastal Zone Development and Resource Management Standards.

The implementation of the LUP's policies and the Coastal Zoning Code standards runs through the entire design process.

Point Reyes Station Community Plan

The subject property is located within the Point Reyes Station Community Plan area, which was first adopted in 1976 and amended in 1986 and in 2001. Point Reyes Station Community Plan sets forth goals and policies to guide decision-makers in evaluating development based on the unique concerns, characteristics, and challenges.

This subdivision design reflects the desire of the community to sustain the traditional character of Point Reyes Station as the commercial hub for rural West Marin and as a place of full-time residence for people preferring a predominantly rural life style.

5.0 Design Constraints

A. Regulatory Constraints

1) Maximum feasible concentration of clustered development

The Marin County Code Section 20.65.05 requires that clustered development shall be located both to provide for the retention of the maximum amount of land in agricultural use and to protect important wildlife habitat areas.

2) ESHA 100 feet buffers

The Categorical Exclusion Order E-81-6 sets the condition that no development is exempt within one hundred feet, measured horizontally, from the high water mark of any coastal body of water, stream, wetland, estuary, or lake (i.e., Environmentally Sensitive Habitat Areas (ESHA)), regardless of whether such coastal water are depicted on the exclusion map, or not. The delineated total ESHA 100 ft buffer areas account for 38% of the total site area.

3) Lot size restrictions

The Marin County Local Coastal Program Land Use Plan has clear restrictions on the lot size for C-AG3 use (1 unit per 1 to 9 acres). The lot size of each unit cannot be less than 1 acre and cannot be more than 9 acres.

4) On-site wastewater treatment restrictions

The Marin County Local Coastal Program Land Use Plan (Policy C-PFS-9) has preference for on-site individual sewage disposal systems. This preference requires an individual sewage disposal system serving a building or buildings to be located on the same building site, lot, or parcel as the building(s).

5) Open space easements restrictions

The Marin County Code Section 20.65.050 requires that agricultural land not presently in production shall be preserved to the maximum extent feasible as undeveloped private open space to be made available on a lease basis in the future for compatible agricultural uses.

6) Affordable housing restrictions

- Twenty percent of the total number of dwelling units or lots within a subdivision shall be developed as, or dedicated to, affordable housing.
- All required affordable housing units on-site shall be disbursed throughout the development.
- The affordable housing units shall contain, on average, the same number of bedrooms as the market rate units in a residential development. This means that the lot size of affordable housing cannot be smaller than the lot size of market-rate housing, that is,

the lot size of each affordable housing unit must be between 1 acre and 9 acres in the C-AG3 land use designation area.

Design challenges to meet all the regulatory requirements

- The land is broken by four steep canyons. The total ESHA 100 ft buffer areas of the canyons account for 38% of the total site area. The new driveways in the community must not cross or touch these 100 ft buffers. Detouring around these ESHA buffer areas makes efforts to shorten the driveways extremely difficult. In other words, the driveways in the community have to be designed to be relatively long. In addition, the proposed building pads must be planned outside of these ESHA buffer areas.
- Lot size restrictions of 1 unit/1-9acres in the C-AG3 area would limit the concentration of clustering development.
- The preference of on-site individual sewage disposal systems under the Marin County Local Coastal Program Land Use Plan (Policy C-PFS-9) would also greatly limit the concentration of clustering development.
- Given the above challenges and regulatory requirements, the design team collectively developed the preliminary subdivision plan that meets the requirements of concentration of clustering layout while meeting all the regulatory requirements.

B. Physical (Slope) Constraints

The site is on a hillslope with relatively steep slopes (the average slope is about 28.5%). This slope constraint would pose challenges to the site development.

a) The influence of topography on driveway design

The Marin County Code Section 24.04.280 requires that the maximum gradient measured along the centerline should not be steeper than eighteen percent and shall not be steeper than twenty-five percent. In order to meet this requirement and considering that the driveways must bypass the ESHA 100 ft buffer areas, the driveways layout can only be selected in areas with relatively gentle surface slopes.

b) The influence of topography on house location

The location of houses is also affected by the terrain. From the topographic map of the site, the relatively flat areas are mostly at high elevations, and the building pads should be selected in relatively flat areas to the extent possible. This results in the concentration of houses in relatively high elevations.

c) The influence of topography and soil properties on septic tank locations

Septic tanks have very high requirements for soil properties and surface slope, which brings great challenges to the site selection and concentration of clustering of septic tanks.

6.0 Design Elements and Study Reports

The following is a list of the design elements and study reports for the Project at this time:

- Design Plans (including Site Plan, Grading Plan, Constraints Map, OWTS and Drainage Plans, Utilities Plan, and Tentative Map)
- Project Description and Design Basis
- Boundary Survey
- Topographic Survey
- Affordable Housing Plan
- Attorney’s Letter Report Regarding the Project’s Eligibility for Coastal Permit Exclusion
- Biological Site Assessment Report
 - Aquatic Resources Delineation Report
 - Focused Species Assessment
 - USACE’s Preliminary Jurisdictional Determination Pursuant to Section 404 of the Clean Water Act
- Culture Resources Assessment Report
- Arborist Report
- Preliminary Geotechnical Feasibility Study Report
- Drainage Study Report
- Stormwater Control Plan
- Preliminary Onsite Wastewater Treatment System (OWTS) Analysis Report
- Traffic and Circulation Study Report
- NRCS Custom Soil Resource Report
- NMWD’s Letter of Intent (Evidence of Water Supply)
- Proposed Preliminary Conceptual Water System Design and Management Plan
- Preliminary Title Report

7.0 Preliminary Layout Plan

Figure 3 shows the preliminary layout plan. The 82.32-acre subject property is proposed to be subdivided into 37 parcels for future single-family residential development. Of the 37 parcels, 5 parcels are designated as affordable housing. The subdivision considered a density bonus of 35% after meeting the 20% affordable housing requirement. The preliminary plan shown in Figure 3 indicates that individual lots will range between about 1.02- and 8.2-acres, meeting the density requirement for the C-AG3 land use designation (1 unit per 1 to 9 acres).

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Access to the development will be provided via an eastward extension of the existing Water Tank Road, via a new access driveway from State Route 1 a few hundred yards north of the existing Water Tank Road, and a new driveway extending from Point Reyes-Petaluma Road in the southeast corner of the site (see Figure 3).

Below provides explanations on the preliminary layout plan.

- **Land Horizontal Features**

Four ESHA 100 ft buffer areas split the land into an extremely irregular and twisted lot. The ESHA protection area in the middle of the land divide the land into two parts (southern and northern) that cannot be connected by driveways (see Figure 3). The project does not propose any development within the ESHA 100 ft buffer areas.

The area that can be connected by driveways in the northern area is relatively large, while that in the southern area is very small.

- **Driveways Layout**

Because the driveways must bypass the stream protection areas, it is impossible to design the driveways into a more regular route, and it also inevitably increases the length of the driveways.

There are three main driveways, of which two driveways are located in the northern area of the land and intersect each other.

The main entrance is connected to the existing Water Tank Road. This driveway enters and exits at the main gate of Water Tank Road.

The westernmost driveway intersects with CA-1. Exit from this driveway could also act as an emergency exit.

The southern area has one independent driveway that intersects with Point Reyes-Petaluma Road.

All the centerline radius of the driveways is more than or equal to forty feet.

All driveways are designed to have slopes not more than 18% and meet the following width requirements specified in the Marin Coastal Zoning Code section 20.65.030.A.

- The minimum improved width of a driveway serving a single dwelling unit is 12 feet.
- The minimum improved width of a driveway serving two to six dwelling units is 16 feet.
- A driveway which serves more than six dwelling units shall be considered equivalent to a private road and designed accordingly. The minimum width is 20 ft.

- **Land Subdivision**

Refer to the Affordable Housing Plan on how the 5 affordable housing units and the 37 total units were derived.

- **Building Pad Locations**

All proposed building pads are located only along the proposed driveways, which is to improve the accessibility of the lots and to maximize the feasible concentration of clustering.

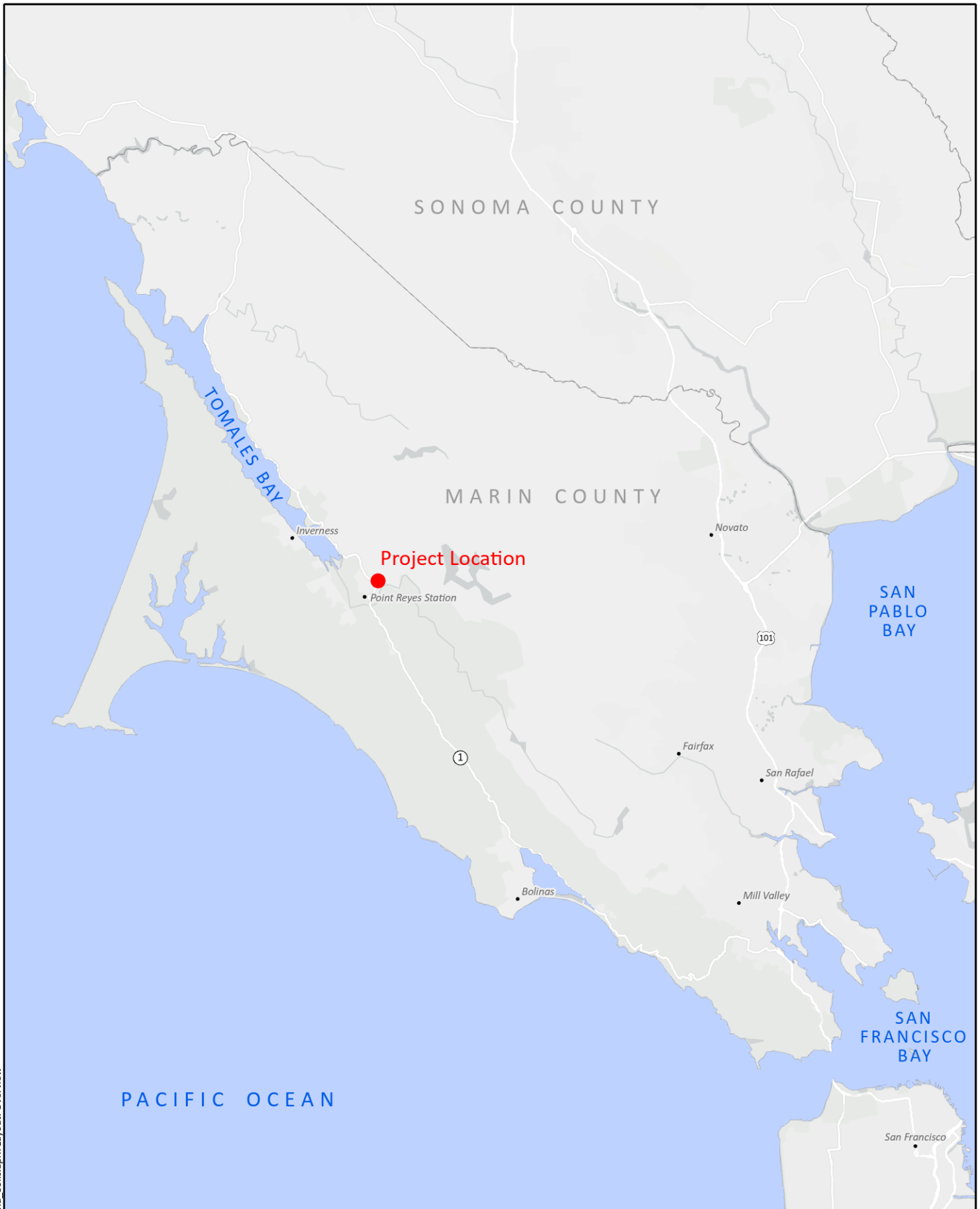
- **Land Accessibility**

All proposed lots are easily accessible.

- **Grading Plan**

This plan sets out a grading plan for the proposed driveways. Both cutting slope and filling slope are not steeper than 50%. In some areas due to site conditions, the filling slope may be greater than 50%. In this case, special techniques for stabilizing the slope will be provided.

Figure 1a



Path: J:\n2904\Stormwater_and_Soils.aprx Layout: Overview



Note:

**PROJECT LOCATION
POINT REYES STATION, CA**

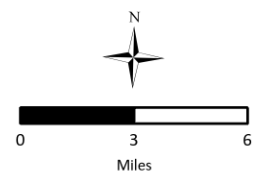
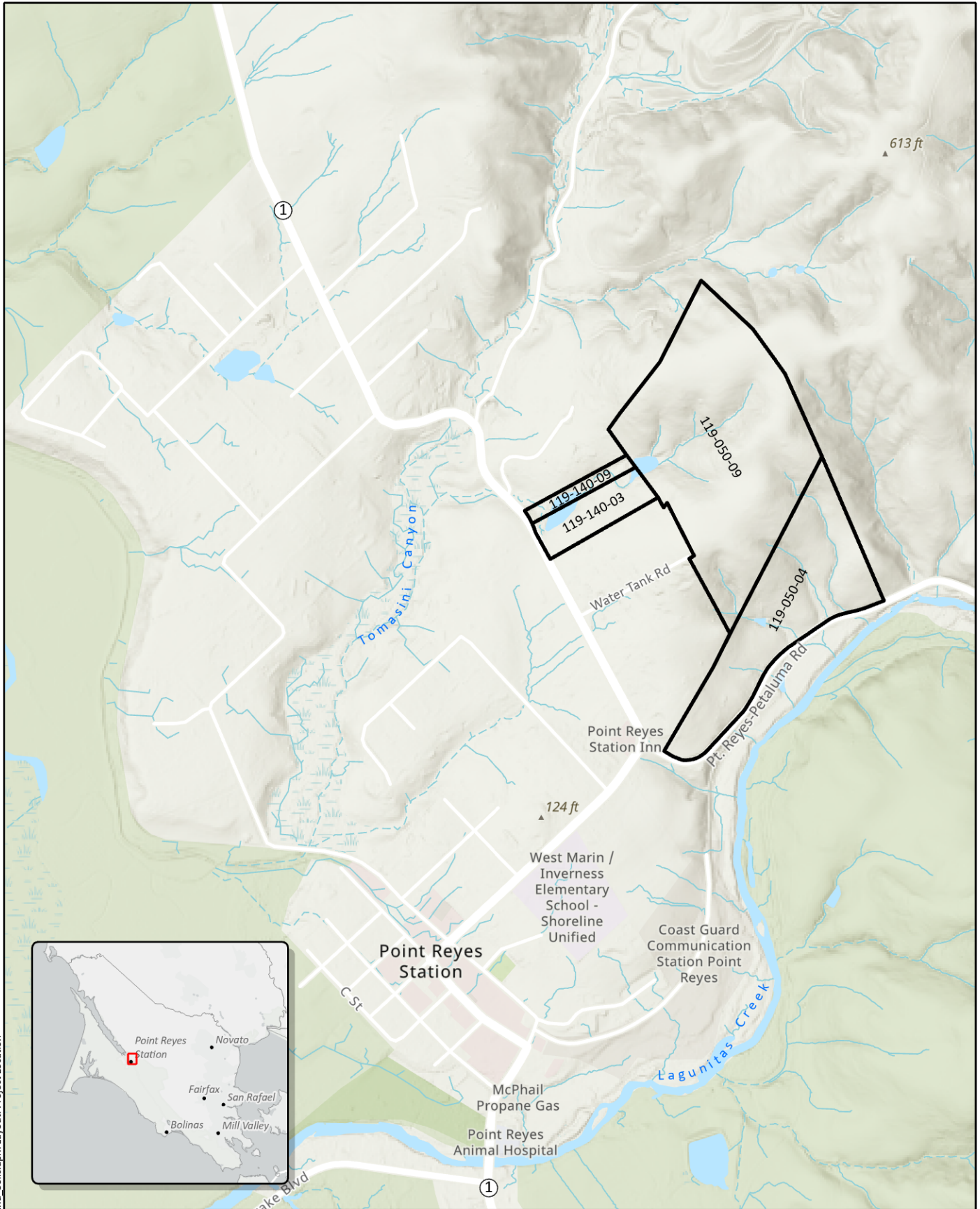


Figure 1b

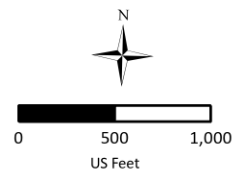


Path: J:\n2904\Stormwater_and_Solls.aprx Layout: Project Location

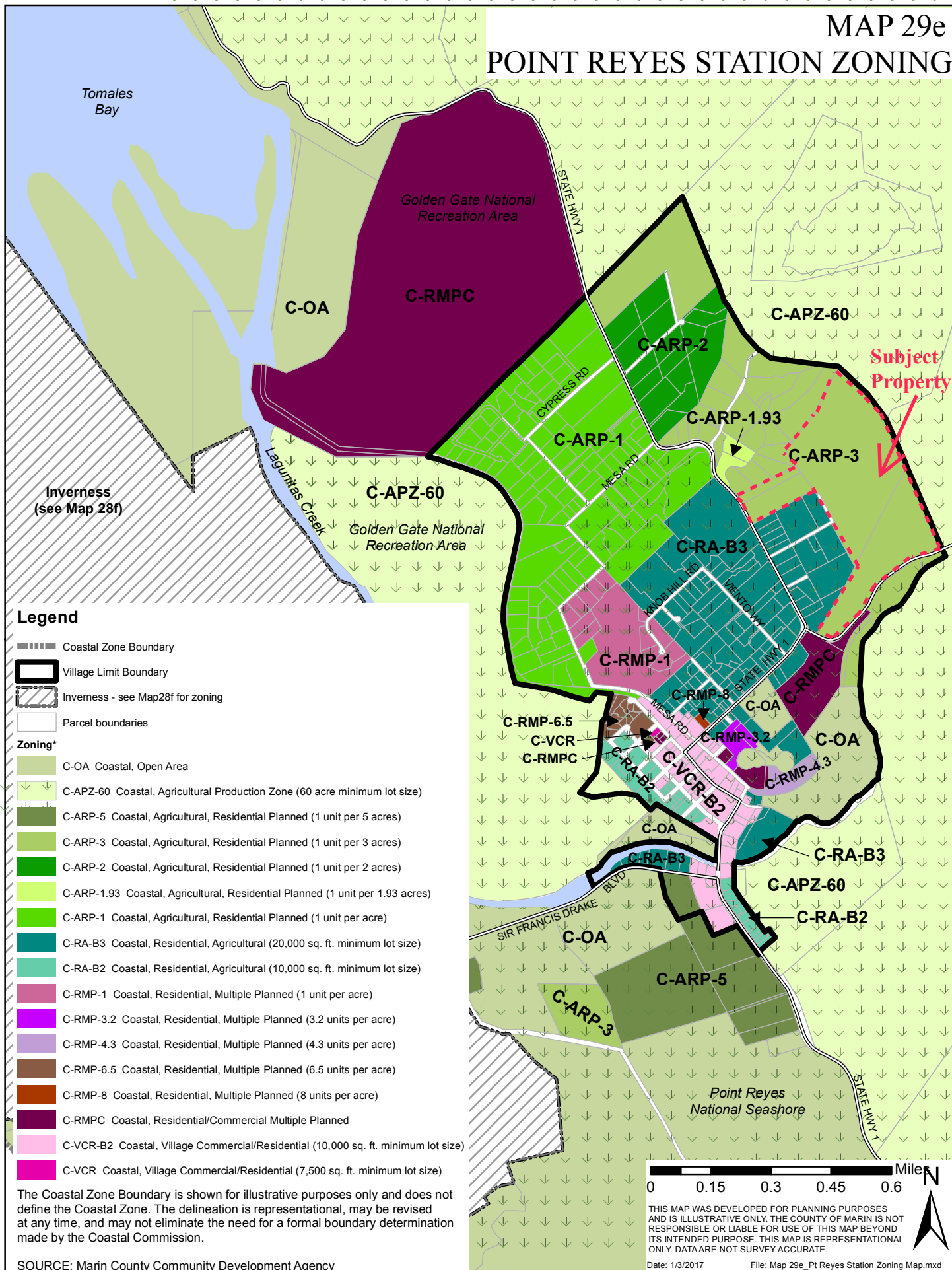


**PROJECT VICINITY AND EXISTING PROPERTY PARCELS
POINT REYES STATION, CA**

Note: Parcel Boundaries from Marin County.



MAP 29e POINT REYES STATION ZONING



0 0.15 0.3 0.45 0.6 Miles

THIS MAP WAS DEVELOPED FOR PLANNING PURPOSES AND IS ILLUSTRATIVE ONLY. THE COUNTY OF MARIN IS NOT RESPONSIBLE OR LIABLE FOR USE OF THIS MAP BEYOND ITS INTENDED PURPOSE. THIS MAP IS REPRESENTATIONAL ONLY. DATA ARE NOT SURVEY ACCURATE.

MAP 19e

POINT REYES STATION LAND USE POLICY MAP

The Coastal Zone Boundary is shown for illustrative purposes only and does not define the Coastal Zone. The delineation is representational, may be revised at any time in the future, is not binding on the Coastal Commission, and may not eliminate the need for a formal boundary determination made by the Coastal Commission.

Legend

- Coastal Single Family
 - C-SF5 2-4 units/acre
 - C-SF4 1-2 units/acre
- Coastal Multi Family
 - C-MF3 5-10 units/acre
 - C-MF2 1-4 units/acre
- Coastal Planned Residential
 - C-PR 1 unit/1-10 acres
- Coastal Neighborhood Commercial / Mixed Use
 - C-NC 1-20 units/acre
F.A.R. = 0.30 TO 0.50
- C-OS Coastal Open Space
- Coastal Agricultural
 - C-AG3 1 unit/1-9 acres
 - C-AG1 1 unit/31-60 acres
- Village Limit Boundary

F.A.R. = Floor Area Ratio

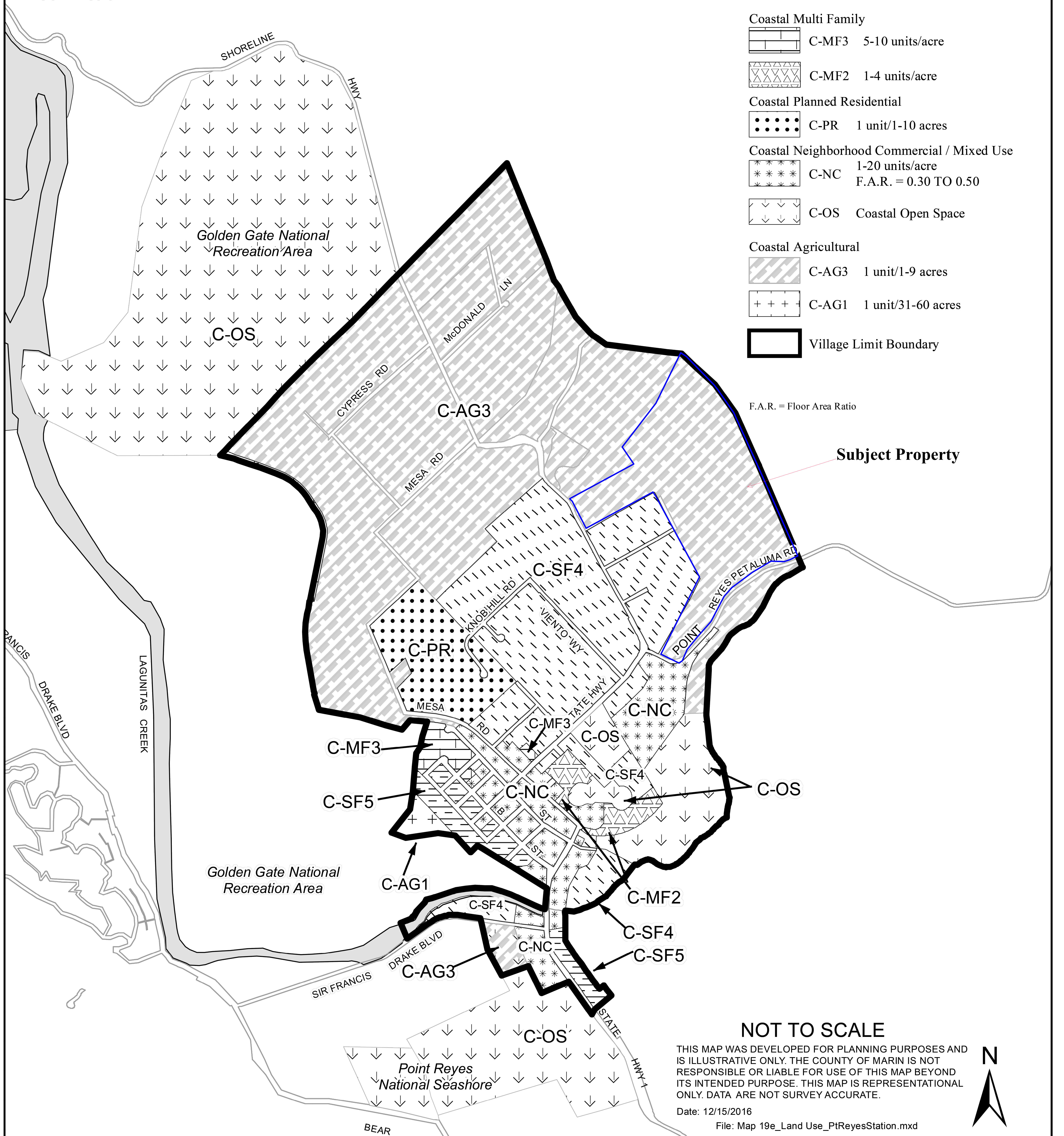
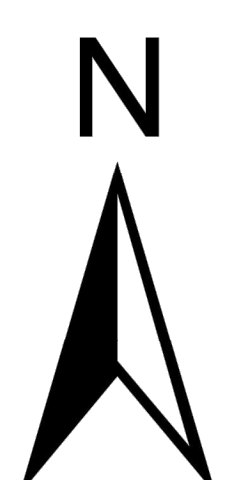
Subject Property

NOT TO SCALE

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Date: 12/15/2016

File: Map 19e_Land Use_PtReyesStation.mxd



Legend

-  Land Boundary
-  Parcels 20240824
-  Leftovers
-  ESHA
-  Affordable Housing Pad
-  Market Housing Pad
-  Common Area
-  Public Area
-  Cutting Area 20240824
-  Filling Area 20240824
-  Balanced Area
-  Retaining Wall
-  Driveway CenterLine
-  Driveway Curb Line
-  Entrance Centerline
-  Bike Lanes
-  Public Road

Figure 3
Preliminary Subdivision

