

# DEPARTMENT OF PUBLIC WORKS

Quality, Excellence, Innovation

Christopher Blunk, P.E.  
DIRECTOR

June 1, 2026

To: All holders of CONTRACT DOCUMENTS for:

## Nicasio Valley Road Paving Project County Project No. 41EP2601D

Administration  
PO Box 4186  
San Rafael, CA 94913-4186  
415 473 6528T  
415 473 3799 F  
415 473 3232 TTY  
CRS Dial 711  
[www.marincounty.org/pw](http://www.marincounty.org/pw)

From: Rachel Calvert  
Senior Civil Engineer  
Department of Public Works

Re: Addendum 3

You are hereby advised of the following revisions and/or clarifications to said Contract Documents:

### SPECIFICATIONS:

Accounting  
Administrative Services  
Airport  
Building Maintenance  
Capital Projects  
Certified Unified Program Agency (CUPA)  
Communications Maintenance  
Disability Access  
Engineering & Survey  
Fleet Operations  
Flood Control & Water Resources  
Land Development  
Procurement  
Real Estate  
Reprographic Services  
Road Maintenance  
Stormwater Program  
Transportation & Traffic Operations  
Waste Management

1. The Engineer's Estimate has been revised to \$1,100,000.

*The Engineer's estimate for this project is **\$1,100,000**.*

Remove page 4 and replace with Page 4[Addendum 3] included with this addendum.

2. First Counted working day has been changed from being no later than 15 days from award, to no later than 20 days from award.

*The first counted Working Day shall be the day the Contractor commences on-site work, or the **twentieth (20th)** business day from the date of written notification emailed by the Department of Public Works that the contract has been awarded, whichever occurs first.*

Remove page 11 and replace with Page 11[Addendum 3] included with this addendum.

3. Several bid quantities have been revised.

Remove page 19 and replace with Page 19[Addendum 3] included with this addendum.

4. Requirement to pave some areas on the same day as the grind has been clarified to identify only those areas being ground below the grade of the cape seal.

***Areas ground below the chip seal subgrade shall not be left unpaved at the end of the work shift unless otherwise approved by the Engineer.***

*Where grinding extends below the required chip seal grade, the Contractor shall place HMA and restore the surface to the required chip seal grade on the same working day that the grinding is performed. Areas requiring same-day paving include, but are not limited to, digouts, mill-and-fill areas below chip seal grade, shoulder repairs, new shoulder construction, and any other locations where grinding results in a surface below the required chip seal grade.*

*Areas ground to the chip seal subgrade shall be chip sealed within three calendar days of the grind. Where grinding is performed solely to prepare for direct chip seal application, no HMA replacement is required prior to chip seal placement. Such areas include, but are not limited to, edge grinding and grinding intended to provide a smooth surface for direct chip seal application such as the Lucas Valley Road intersection.*

Remove page 69 and replace with Page 69[Addendum 3] included with this addendum.

5. Clarify daily sweeping to include holidays and weekends.

*Sweep no less than once per **calendar** day until surface has been paved.*

Remove page 94 and replace with Page 94[Addendum 3] included with this addendum.

#### **PLANS:**

6. Sheet 3: Pavement Rehabilitation Sta 1+00 to 20+10

Additional shoulder widening details have been revised.

Replace Sheet 3: Pavement Rehabilitation Sta 1+00 to 20+10 with Sheet 3: Pavement Rehabilitation Sta 1+00 to 20+10 [Addendum 3] included with this addendum

7. Sheet 4: Pavement Rehabilitation Sta 20+10 to 34+25

Section B and E details have been revised.

Replace Sheet 4: Pavement Rehabilitation Sta 20+10 to 34+25 with Sheet 3: Pavement Rehabilitation Sta 20+10 to 34+25 [Addendum 3] included with this addendum

**8. Sheet 5: Pavement Rehabilitation Sta 34+25 to 55+38**

Station callouts have been added, and section details have been revised.

Replace Sheet 5: Pavement Rehabilitation Sta 34+25 to 55+38 with Sheet 5: Pavement Rehabilitation Sta 34+25 to 55+38 [Addendum 3] included with this addendum

**9. Sheet 6: Detail Sheet**

Detail 7 has been added and detail 6 has been revised.

Replace Sheet 6: Detail Sheet with Sheet 6: Detail Sheet [Addendum 3] included with this addendum

END OF ADDENDUM NO. 3

Approved by and issued by:



Christopher Blunk, P.E.  
Director of Public Works  
County of Marin

Enclosed:

- Specifications
  - Page 4 [Addendum 3]
  - Page 11 [Addendum 3]
  - Page 19 [Addendum 3]
  - Page 69 [Addendum 3]
  - Page 94 [Addendum 3]
  
- Plans
  - Sheet 3: Pavement Rehabilitation Sta 1+00 to 20+10 [Addendum 3]
  - Sheet 4: Pavement Rehabilitation Sta 20+10 to 34+25 [Addendum 3]
  - Sheet 5: Pavement Rehabilitation Sta 34+25 to 55+38 [Addendum 3]
  - Sheet 6: Detail Sheet [Addendum 3]

MARIN COUNTY DEPARTMENT OF PUBLIC WORKS  
SAN RAFAEL, CALIFORNIA

**NOTICE TO BIDDERS**

INVITING SEALED PROPOSALS OF BIDS

\*\*\*\*\*

Notice is hereby given by order of the Marin County Board of Supervisors, that the Director of Public Works will receive bids for:

**Project Name:** NICASIO VALLEY ROAD PAVING PROJECT  
**County Project No.** 41EP2601D

**Bid Opening Date:** June 4, 2026 at 1:00 PM

**Zoom Meeting ID:** 833 8585 3989, Password: ENG2026

<https://us05web.zoom.us/j/83385853989?pwd=JG3K1lgR1apaE73LgQRQaFxrIGocac.1>

Bids must be submitted electronically through [Bid Express®](#), or by submitting sealed paper bids to the Director of Public Works via the administration desk at 3501 Civic Center Drive, Suite 304, San Rafael, CA 94903, **on or before the date and time noted above**, at which time bids will be publicly opened and read aloud on Zoom; Zoom meeting details noted above.

Plans can be obtained by visiting the County of Marin Bid Express home page at <https://www.bidexpress.com/businesses/53528/home>. Bidders can register for a free Bid Express account to view project solicitations; download bid documents; view and add themselves to the plan holder's list and submit bid requests for information.

Paper copies of the bid documents and reasonable accommodations for auxiliary aids will be made available upon request by contacting Sharon Ho, Engineering Division, [Sharon.ho@marincounty.gov](mailto:Sharon.ho@marincounty.gov) or (415) 473-6574.

**Project Description:**

Improvements for this project will generally include asphalt grinding/milling, pavement digouts, hot mix asphalt overlay, traffic striping, traffic marking, traffic signs, shoulder backing and traffic control. The work will take place on Nicasio Valley Road from Lucas Valley Road to MP 2.82 bridge (approximately 1.09 miles north of the Lucas Valley Road intersection) in Nicasio.

The Engineer's estimate for this project is **\$1,100,000**. Bids are required for the entire work described herein.

The Contractor shall possess a valid Class "A" License at time of bid submittal.

To bid on this Project, the Bidder and all subcontractors are required to be registered as a public works contractor with the Department of Industrial Relations pursuant to the Labor Code. The Bidder's and all subcontractors' registration must remain active throughout the term of the Contract.

A Bid Bond, cash, certified check, or cashier's check, in the amount of 10% of the total bid amount payable to the County of Marin shall accompany the Proposal.

Any bids received after the time specified on the Notice to Bidders or as extended via Addendum shall be returned unopened.

- G. TIME LIMIT AND LIQUIDATED DAMAGES: –From the date of emailing of written notification by the Department of Public Works that the contract has been awarded, the Contractor must execute the necessary contract documents and return them to the Department of Public Works within ten (10) business days.

**The first counted Working Day** shall be the day the Contractor commences on-site work, or the **twentieth (20th)** business day from the date of written notification emailed by the Department of Public Works that the contract has been awarded, **whichever occurs first**.

**All work shall be completed within thirty-five (35) working days, which includes working day submittal review period.** Submittals shall be prepared, submitted, and coordinated so as not to delay the progress of the Work. No extension of time will be granted for delays associated with submittals, including resubmittals or incomplete submittals.

The County specifies liquidated damages (Public Contract Code § 10226). Liquidated damages, if any, accrue starting on the 1st day after the expiration of the working days through the day of Contract Completion. The Contractor shall pay to the County the sum of **\$3,000.00** per each and every calendar day of delay in completing the work in excess of the number of working days specified above and as modified by Contract Change Order. It is understood that additional crews may be needed to complete the work within the timeline specified.

- H. LEGAL REQUIREMENTS: - See the special provisions and standard specifications regarding legal relations and responsibility.
- I. SPECIFICATIONS: - Whenever a matter is not covered within the Special Provisions or elsewhere within this contract, the provisions contained within the Standard Specifications of the State of California, Business, Transportation and Housing Agency, Department of Transportation (Caltrans), dated 2022, including any revisions (herein incorporated by reference) shall apply to this contract.

Reference to sections in the standard specification is not a comprehensive listing of the sections which apply to the project. Unless deleted, all sections apply to the project.

- J. CONTRACT BONDS: - The Contractor whose bid is accepted shall furnish the following bonds to the County (at no expense to County), executed by a responsible surety admitted to do business in California in a form acceptable to the County:

- a) Performance Bond
- b) Payment Bond

The Performance Bond shall be in an amount equal to one hundred percent (100%) of the total amount bid by the Contractor in their proposal.

The Payment Bond shall be in an amount equal to one hundred percent (100%) of the total amount bid by the Contractor in their proposal.

<b>SCHEDULE OF BID PRICES</b> <b>NICASIO VALLEY ROAD PAVING PROJECT</b>
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BASE BID					
Bid item	Description	Quantity		Unit price	Amount
1	Mobilization	1	LS	\$	\$
2	Traffic Control and Construction Signs	1	LS	\$	\$
3	Stormwater Pollution Control Program	1	LS	\$	\$
4	Clearing and Grubbing	1	LS	\$	\$
5	Edge Grind	4,100	SY	\$	\$
6	Conform Grind	650	SY	\$	\$
7	Full Width Grind (2"-3")	4,300	SY	\$	\$
8	HMA (Type A)	2,500	TON	\$	\$
9	Asphalt Rubber Chip Seal	21,500	SY	\$	\$
10	Asphalt Digout	14,000	SF	\$	\$
11	Roadway Shoulder Replacement	2,100	SF	\$	\$
12	Shoulder Backing	1,175	TON	\$	\$
13	Adjust Monument Box to Grade	3	EA	\$	\$
14	Adjust Telephone Manhole	2	EA	\$	\$
15	Traffic Striping Detail 19	2,505	LF	\$	\$
16	Traffic Striping Detail 22	2,806	LF	\$	\$
17	Traffic Striping Detail 27B	10,890	LF	\$	\$
18	Traffic Striping Detail 27C	180	LF	\$	\$
19	Traffic Striping Detail 38A	50	LF	\$	\$
20	Limit Line (STOP BAR)	90	LF	\$	\$
21	4" White Stripe	150	LF	\$	\$

center of the straddler and the monument utility box and cover shall be installed per the current Marin Uniform Construction Standard. There shall not be any perceptible difference in elevation between the finished pavement surface and the cover.

When monument destruction is authorized, replacement will be **paid on force account**. Contractor shall replace monuments at their sole cost when destruction was not authorized.

When monument destruction is not authorized, replacement will be **at no cost to the County**.

#### 10.14 **FULL WIDTH GRIND (2"-3"), CONFORM GRIND, AND EDGE GRIND**

**Full Width Grind (2"-3"), Conform Grind and Edge Grind** shall include the removal of asphalt, aggregate base, and native soil necessary to achieve subgrade.

Pavement Grinding shall be in accordance with the applicable provisions of Section 42-3 "Grinding" of Caltrans Standard Specifications.

**Areas ground below the chip seal subgrade shall not be left unpaved at the end of the work shift unless otherwise approved by the Engineer.**

Where grinding extends below the required chip seal grade, the Contractor shall place HMA and restore the surface to the required chip seal grade on the same working day that the grinding is performed. Areas requiring same-day paving include, but are not limited to, digouts, mill-and-fill areas below chip seal grade, shoulder repairs, new shoulder construction, and any other locations where grinding results in a surface below the required chip seal grade.

**Areas ground to the chip seal subgrade shall be chip sealed within three calendar days of the grind.** Where grinding is performed solely to prepare for direct chip seal application, no HMA replacement is required prior to chip seal placement. Such areas include, but are not limited to, edge grinding and grinding intended to provide a smooth surface for direct chip seal application such as the Lucas Valley Road intersection.

The contractor shall protect the subgrade including, but not limited to, avoiding rain on the subgrade, avoiding unnecessary heavy loads on the subgrade, identifying signs of subgrade cracking or 'pumping', keeping trucks off those areas and immediately notifying the engineer.

Pavement grinding operations shall not commence until all existing traffic striping and all street surface facilities/features including utility castings and boxes, survey monuments and benchmark within the areas to be ground have been "tied out", with reference points, by the Contractor and noted to the Engineer.

Pavement Grinding of asphalt concrete pavement shall be performed by the cold planning method. The cold planning machine shall be capable of planning the pavement without requiring the use of a heating device to soften the pavement during or prior to the planning operation.

Pavement Grinding must be in accordance with the applicable provisions of the specifications and shall involve: Grinding an adequate depth so the finished grinding surface is the depth below finished grade as shown on the plans, across the roadway between the lip of gutter and lip of gutter, or edge of pavement, with cross slopes that match existing or as directed by the plans.

The final cut must result in a uniform surface conforming to the plans. The outside lines of the planed area must be neat and uniform. Planning asphalt concrete pavement operations must be performed without damage to the surfacing to remain in place.

an extended period, the cover material spreader (chip-box) shall be moved ahead or off the chip seal surface so that all cover material may be immediately rolled. Three complete passes shall be made with the pneumatic rollers.

The pneumatic tire rollers shall be operated in front of the steel wheel roller.

### **Sweeping**

1. Sweep as often as necessary for safety and environmental protection.
2. Sweep no less than once per **calendar** day until surface has been paved.
3. Perform a final sweeping immediately before paving. The final sweeping must not dislodge aggregate.

### **Flush Coat**

Flush Coat shall conform to section 37-4 “Flog Seals and Flush Coats: of the Standard Specifications and these provisions.

Applying a flush coat includes applying a fog seal coat followed by sand. Place Flush Coat within one day of applying rubber chip seal unless it is covered by hot mix asphalt within three days of applying rubber chip seal.

### **MEASUREMENT AND PAYMENT**

**Asphalt Rubber Chip Seal** shall be measured on a square yard (SY) basis of actual surface area covered per the plans and specifications and as directed by the Engineer.

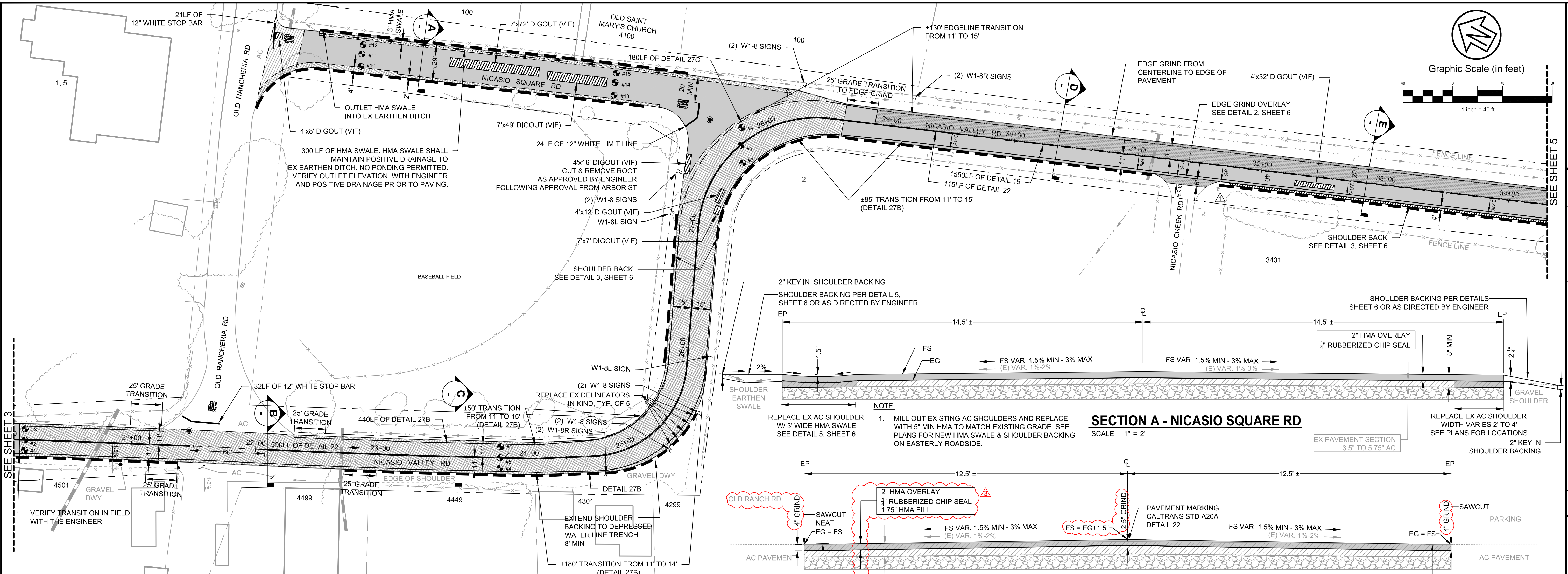
The price paid per square yard for **Asphalt Rubber Chip Seal** shall include full compensation for furnishing all labor, materials, tools and equipment necessary to perform the work involved in testing and furnishing the mix design, preparation and placing Asphalt Rubber Chip Seal including sweeping, cleaning cracks, rolling, flush coat, protecting utilities, protecting the work, post sweeping, temporary delineation, staging materials and all incidental work or materials necessary to complete the work as specified in these Special Provisions and indicated elsewhere in the contract documents.

### **10.19 TACK COAT**

Tack Coat shall be SS-1h, unless the pavement is to receive a paving fabric or mat, and use and application rates shall be in accordance with the applicable provisions of Section 39-2.01C(3)(f) and shall comply with the physical properties found in Section 94-1.02, Anionic Asphaltic Emulsion Requirements Tables, of the Standard Specifications. **Dilution of the mixture with water will not be acceptable.**

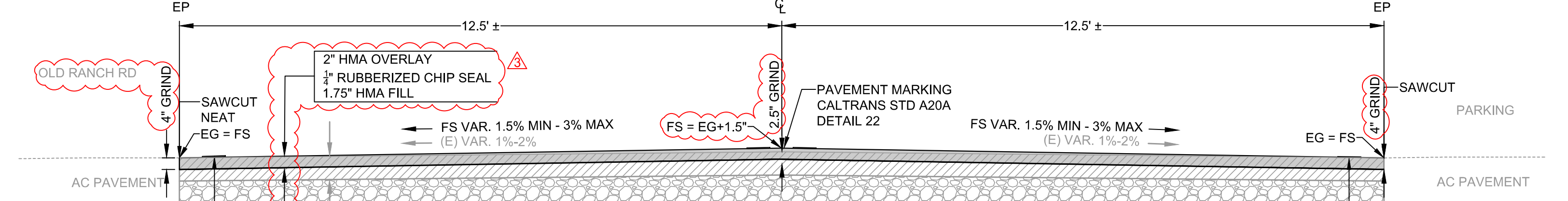


Graphic Scale (in feet)  
1 inch = 40 ft.

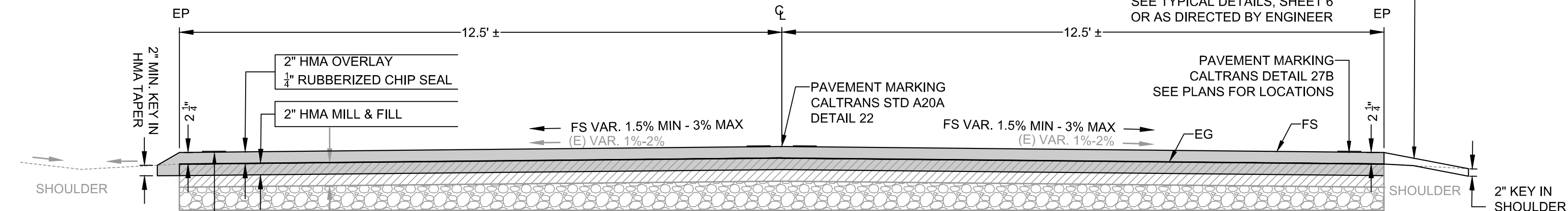


**NICASIO VALLEY RD - STA 20+10 to 34+25**  
SCALE 1 INCH = 40 FT

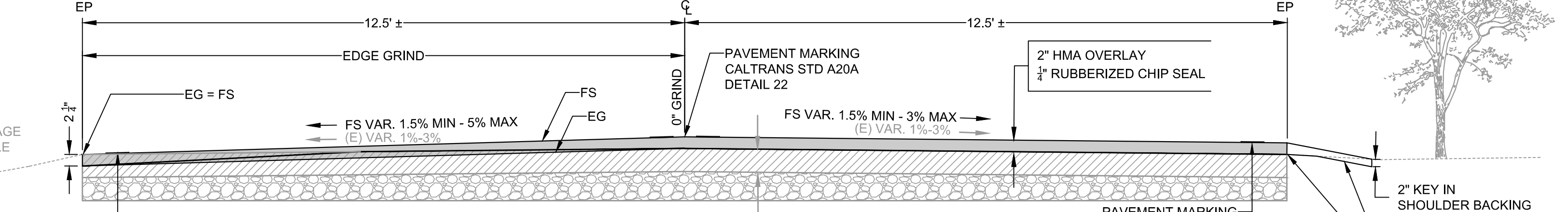
**SECTION A - NICASIO SQUARE RD**  
SCALE: 1" = 2"



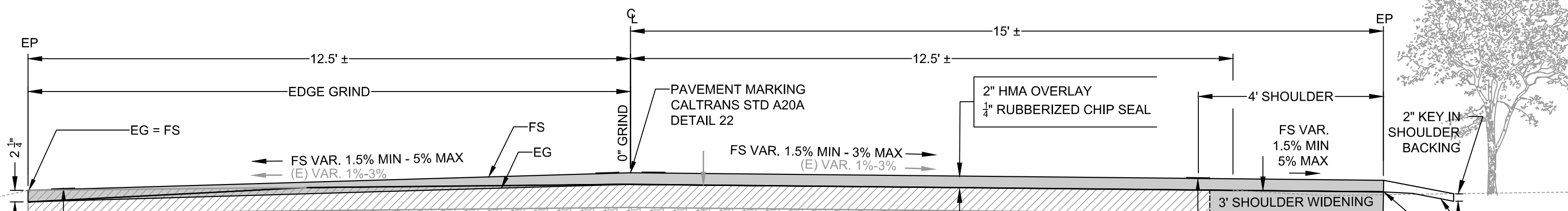
**SECTION B - STA 21+34 to 22+30**  
SCALE: 1" = 2"



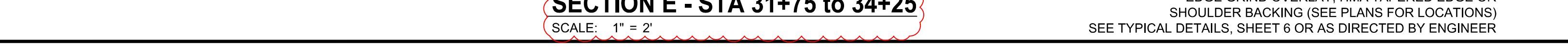
**SECTION C - STA 20+10 to STA 21+34 & 22+30 to 28+62**  
SCALE: 1" = 2"



**SECTION D - STA 28+62 to 31+75**  
SCALE: 1" = 2"



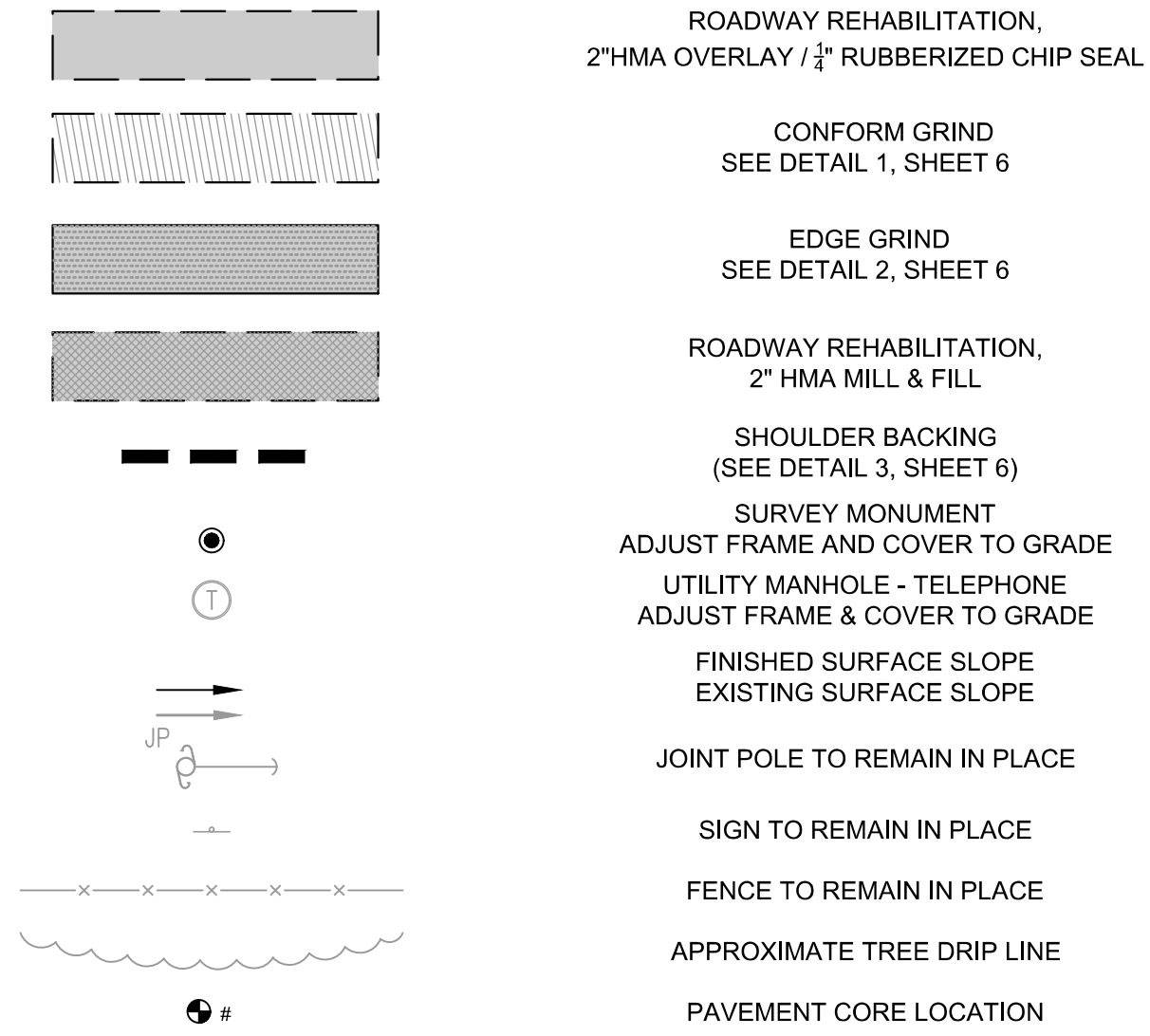
**SECTION E - STA 31+75 to 34+25**  
SCALE: 1" = 2"



**GENERAL NOTES**

- PAVEMENT STRIPING AND MARKINGS PER CALTRANS STANDARD PLANS.
- ADJUST ALL UTILITY FRAMES AND COVERS TO GRADE.
- ADJUST ALL SURVEY MONUMENT COVERS TO GRADE PER UCS DWG 300.
- APPLY TACK COAT BETWEEN NEW & EX. PAVEMENT.
- SAWCUT AND CONFORM NEAT AT ENDS OF ALL ROADWAYS AND ASPHALT DRIVEWAY APPROACHES.
- SEE PLANS FOR SHOULDER BACKING LOCATIONS.
- WHERE SHOULDER BACKING CANNOT BE PLACED DUE TO CONSTRAINT, CONSTRUCT A 30° - 35° (57.7% - 70.0%) HMA TAPERED EDGE.
- PROVIDE VARIABLE DEPTH GRINDING TO ACHIEVE THE PROPOSED SUBGRADE SO THAT A UNIFORM PAVEMENT THICKNESS PRODUCES A FINISHED SURFACE CONFORMING TO THE GRADES SHOWN.
- GRIND PLANE SHALL BE PARALLEL TO THE PROPOSED FINISHED SURFACE.
- WHERE SHOWN ON THE PLANS, CONFORM FINISHED GRADE TO EXISTING GRADE AT EDGE OF PAVEMENT (EP), WHERE VERTICAL OFFSETS FROM EXISTING EP ARE SHOWN, MATCH THOSE ELEVATIONS, INCLUDING OFFSETS OF 0 TO 2.25 INCHES ABOVE EXISTING GRADE, AS SHOWN.
- PROVIDE A SMOOTH, CONTINUOUS CROSS SLOPE FROM CENTERLINE (CL) TO EP WITHIN THE RANGES SHOWN ON THE PLANS. MAINTAIN POSITIVE DRAINAGE WITHOUT PONDING.
- SEE SECTION 10.15 HOT MIX ASPHALT OF SPECIAL PROVISIONS FOR REQUIREMENTS.

**LEGEND**



**PAVEMENT CORE RESULTS**

LOCATION	HMA THICKNESS (INCHES)
#1	6.0"
#2	6.5"
#3	5.75"
#4	5.5"
#5	6.0"
#6	5.5"
#7	4.5"
#8	6.25"
#9	6.0"
#10	5.75"
#11	4.5"
#12	5.5"
#13	5.0"
#14	3.75"
#15	3.5"

NICASIO VALLEY ROAD PAVING PROJECT  
PAVEMENT REHABILITATION  
STA 20+10 to STA 34+25  
3501 CIVIC CENTER DRIVE, SAN RAFAEL, CA 94903

Town of Nicasio  
County Of Marin  
State Of California

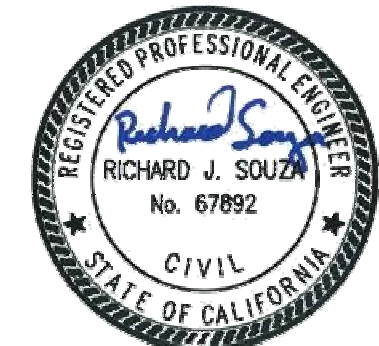
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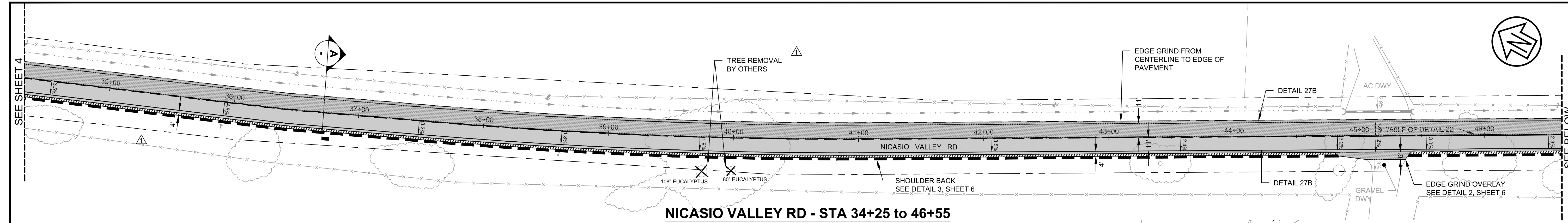
Rev	Date	Description	Drawn	Checked
1	6/11/2026	ADDENDUM 3	RJS	RS
2	5/22/2026	ADDENDUM 1	RJS	RS
3	5/12/2026	BID SET	RJS	RS

NICASIO VALLEY ROAD PAVING PROJECT  
PAVEMENT REHABILITATION  
STA 34+25 TO STA 55+38  
3501 CIVIC CENTER DRIVE, SAN RAFAEL, CA 94903

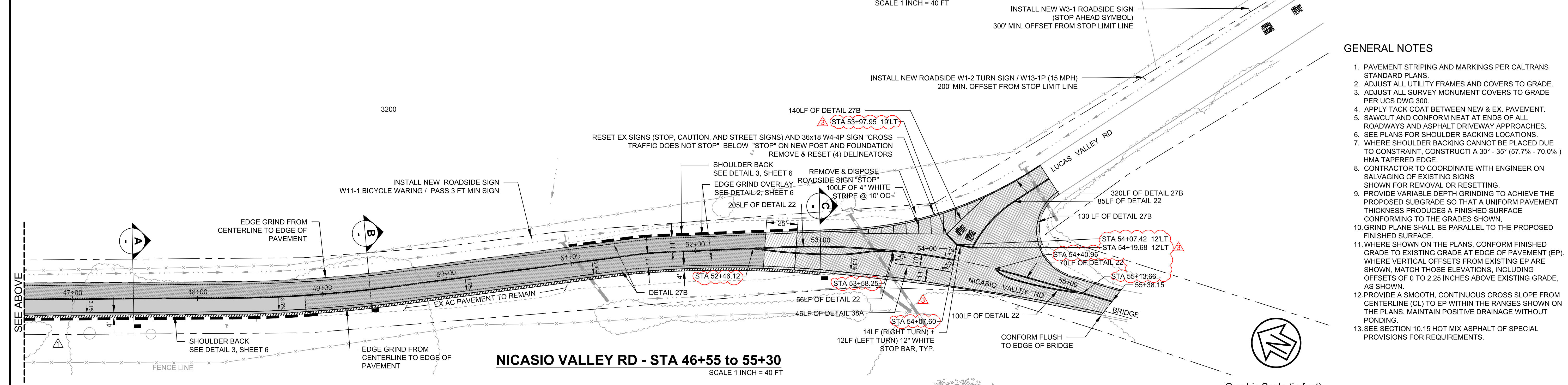
Town of  
**Nicasio**  
County Of  
**Marin**  
State Of  
**California**

Prepared Under the Direction of:  


Sheet  
**5**  
Scale: 1 inch = 40 ft.  
Date: May 12, 2026  
County Plan No. 41EP2601D  
Sheet No. 5 of 6

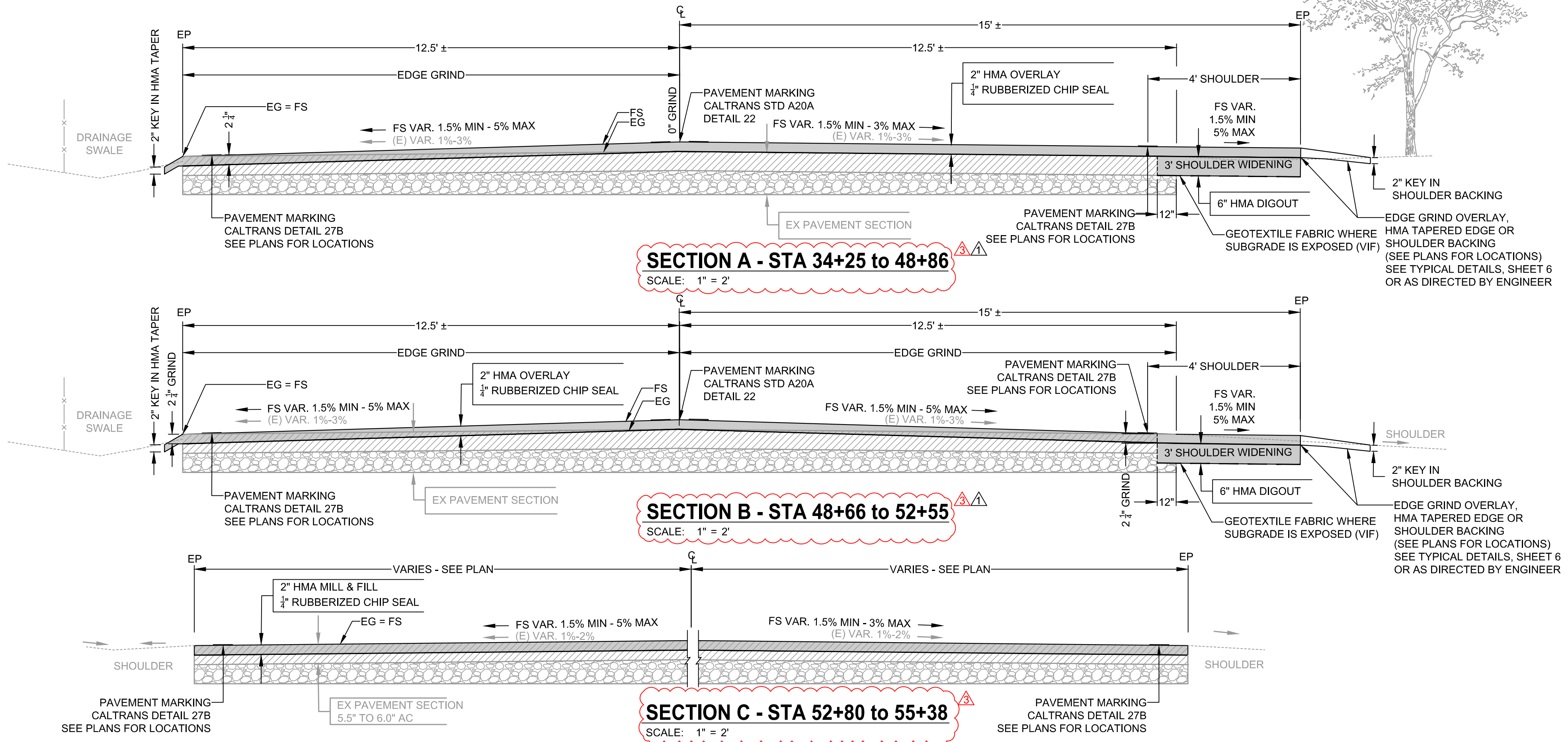
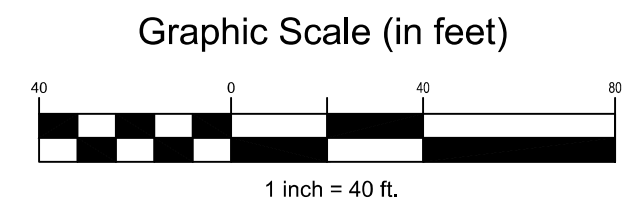


**NICASIO VALLEY RD - STA 34+25 to 46+55**  
SCALE 1 INCH = 40 FT















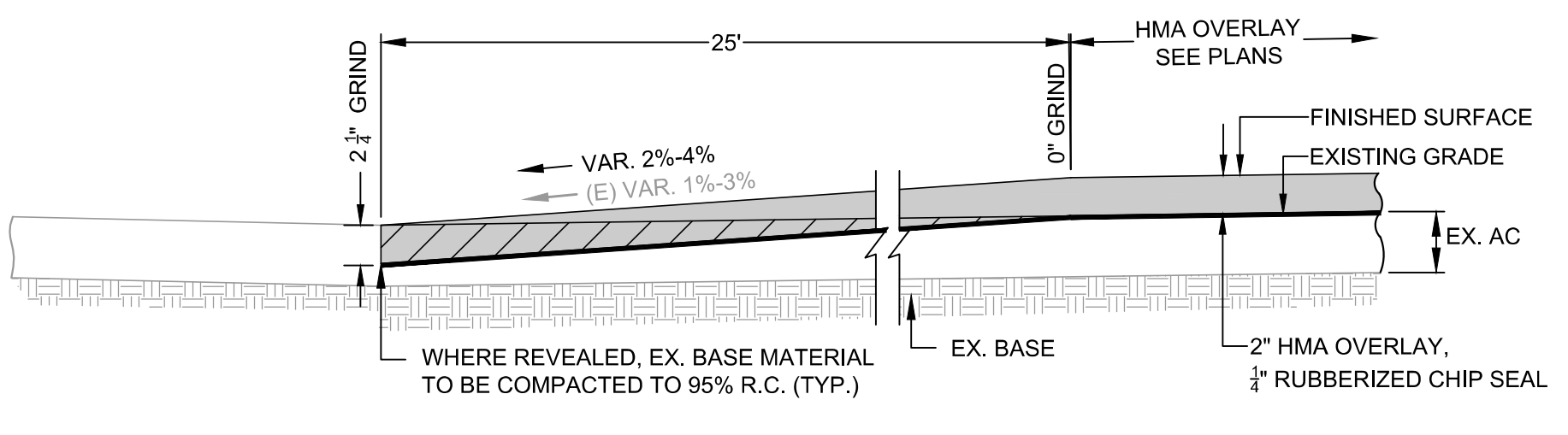
**NICASIO VALLEY RD - STA 46+55 to 55+30**  
SCALE 1 INCH = 40 FT

- GENERAL NOTES**
- PAVEMENT STRIPING AND MARKINGS PER CALTRANS STANDARD PLANS.
  - ADJUST ALL UTILITY FRAMES AND COVERS TO GRADE.
  - ADJUST ALL SURVEY MONUMENT COVERS TO GRADE PER UCS DWG 300.
  - APPLY TACK COAT BETWEEN NEW & EX. PAVEMENT.
  - SAWCUT AND CONFORM NEAT AT ENDS OF ALL ROADWAYS AND ASPHALT DRIVEWAY APPROACHES.
  - SEE PLANS FOR SHOULDER BACKING LOCATIONS.
  - WHERE SHOULDER BACKING CANNOT BE PLACED DUE TO CONSTRAINT, CONSTRUCT A 30" - 35" (57.7% - 70.0%) HMA TAPERED EDGE.
  - CONTRACTOR TO COORDINATE WITH ENGINEER ON SALVAGING OF EXISTING SIGNS SHOWN FOR REMOVAL OR RESETTING.
  - PROVIDE VARIABLE DEPTH GRINDING TO ACHIEVE THE PROPOSED SUBGRADE SO THAT A UNIFORM PAVEMENT THICKNESS PRODUCES A FINISHED SURFACE CONFORMING TO THE GRADES SHOWN.
  - GRIND PLANE SHALL BE PARALLEL TO THE PROPOSED FINISHED SURFACE.
  - WHERE SHOWN ON THE PLANS, CONFORM FINISHED GRADE TO EXISTING GRADE AT EDGE OF PAVEMENT (EP). WHERE VERTICAL OFFSETS FROM EXISTING EP ARE SHOWN, MATCH THOSE ELEVATIONS, INCLUDING OFFSETS OF 0 TO 2.25 INCHES ABOVE EXISTING GRADE, AS SHOWN.
  - PROVIDE A SMOOTH, CONTINUOUS CROSS SLOPE FROM CENTERLINE (CL) TO EP WITHIN THE RANGES SHOWN ON THE PLANS. MAINTAIN POSITIVE DRAINAGE WITHOUT PONDING.
  - SEE SECTION 10.15 HOT MIX ASPHALT OF SPECIAL PROVISIONS FOR REQUIREMENTS.



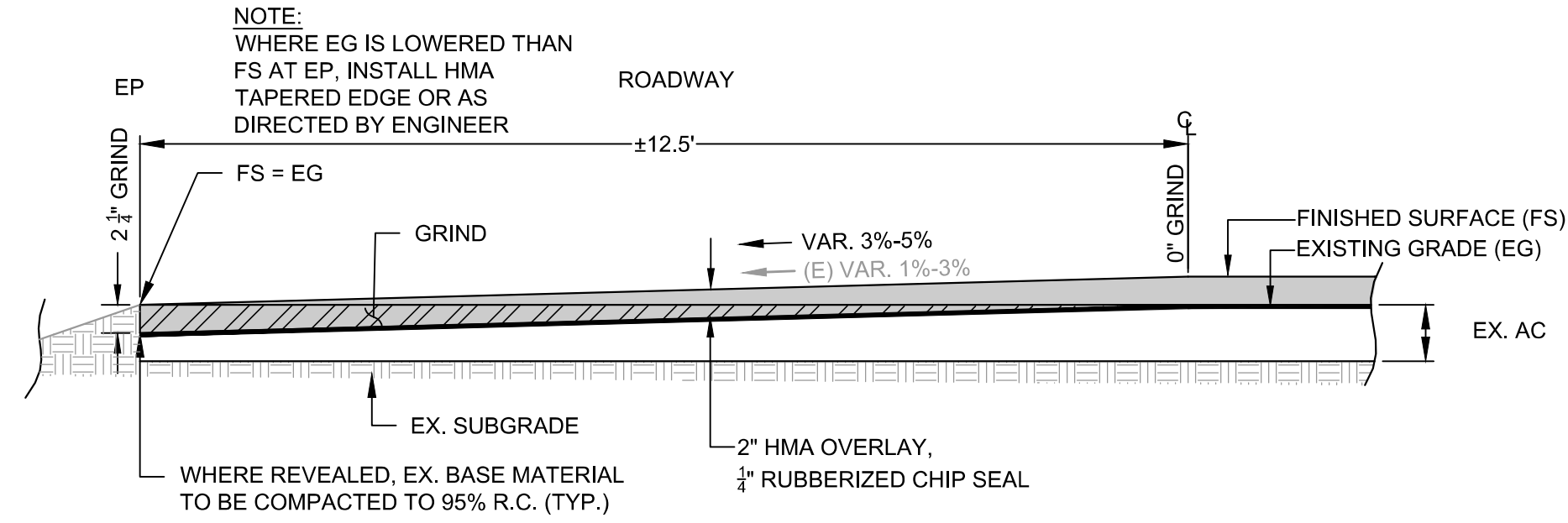
**LEGEND**

-  ROADWAY REHABILITATION, 2" HMA OVERLAY / 3" RUBBERIZED CHIP SEAL
-  CONFORM GRIND SEE DETAIL 1, SHEET 6
-  EDGE GRIND SEE DETAIL 2, SHEET 6
-  ROADWAY REHABILITATION, 2" HMA MILL & FILL
-  SHOULDER BACKING (SEE DETAIL 3, SHEET 6)
-  SURVEY MONUMENT ADJUST FRAME AND COVER TO GRADE
-  UTILITY MANHOLE - TELEPHONE ADJUST FRAME & COVER TO GRADE
-  FINISHED SURFACE SLOPE EXISTING SURFACE SLOPE
-  JOINT POLE TO REMAIN IN PLACE
-  SIGN TO REMAIN IN PLACE
-  FENCE TO REMAIN IN PLACE
-  APPROXIMATE TREE DRIP LINE

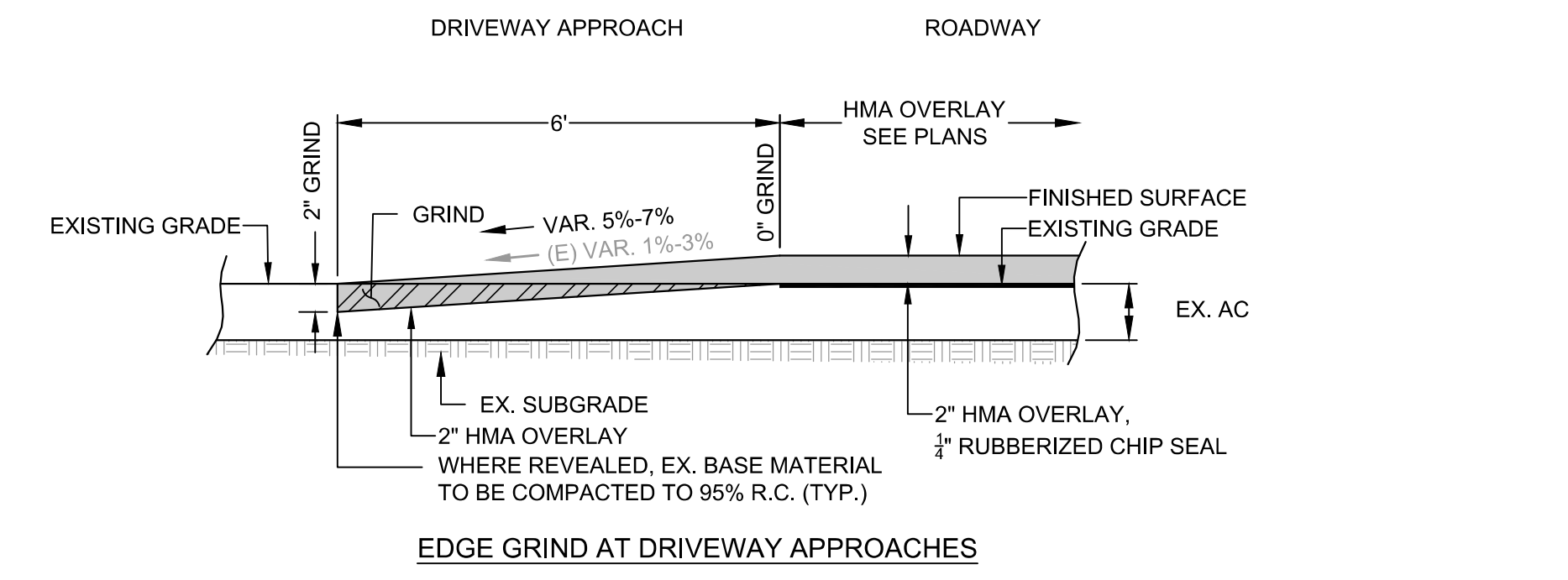


**1 CONFORM GRIND OVERLAY DETAIL**

SCALE: N.T.S



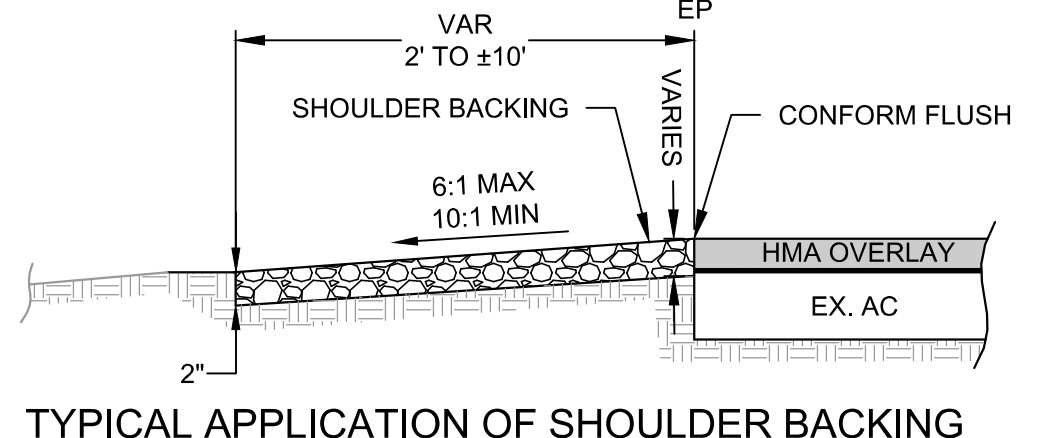
EDGE GRIND IN ROADWAY



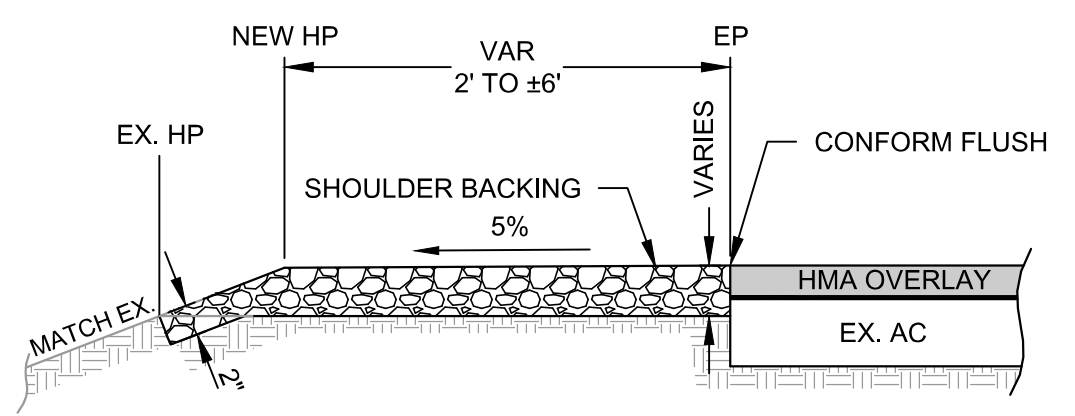
EDGE GRIND AT DRIVEWAY APPROACHES

**2 EDGE GRIND OVERLAY DETAIL**

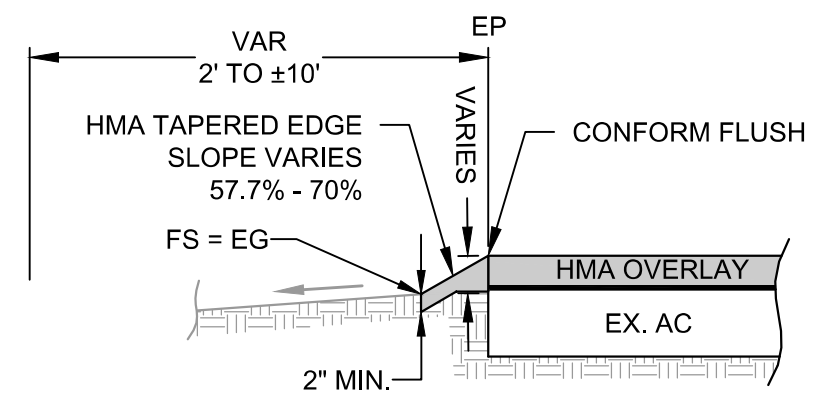
SCALE: N.T.S



TYPICAL APPLICATION OF SHOULDER BACKING



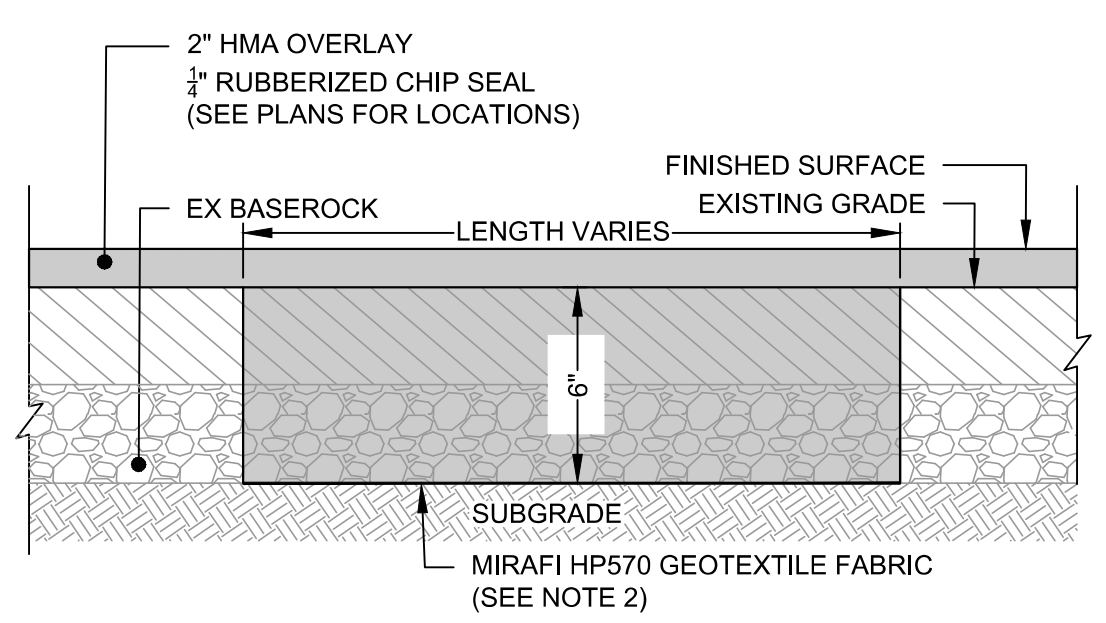
ALTERNATIVE PLACEMENT FOR EXISTING SLOPES STEEPER THAN 6:1



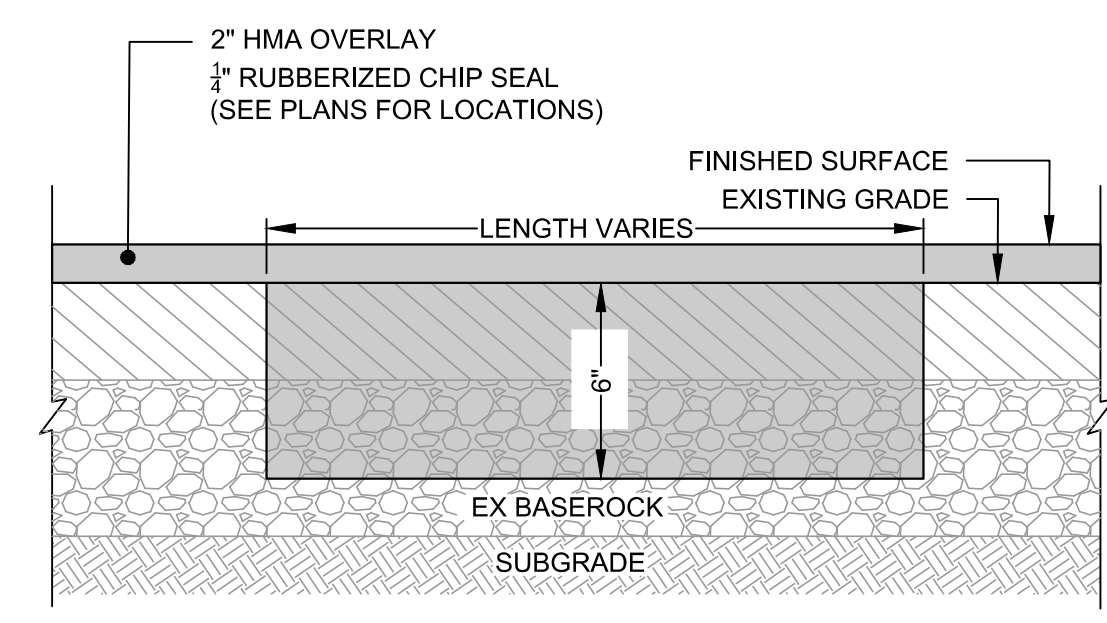
TYPICAL APPLICATION OF HMA TAPERED SHOULDER

**3 SHOULDER BACKING & HMA TAPERED EDGE DETAIL**

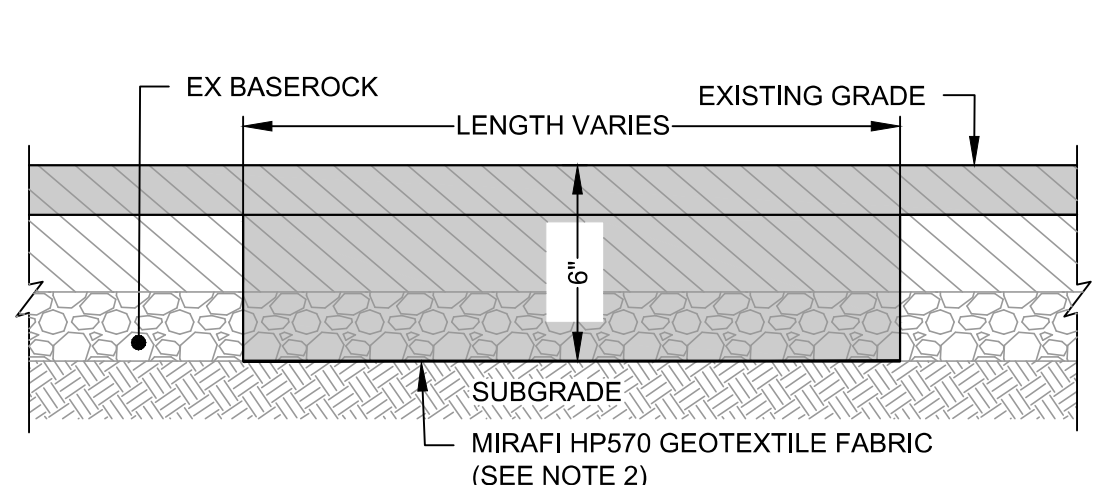
SCALE: N.T.S



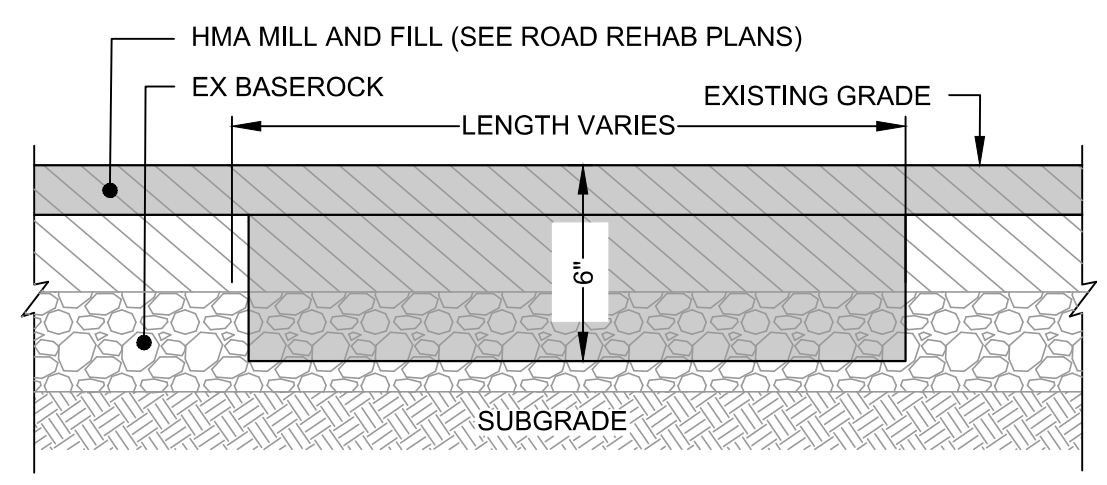
DIGOUT IN HMA OVERLAY LOCATIONS WITH FABRIC



DIGOUT IN HMA OVERLAY LOCATIONS WITHOUT FABRIC



DIGOUT IN HMA MILL & FILL LOCATIONS WITH FABRIC



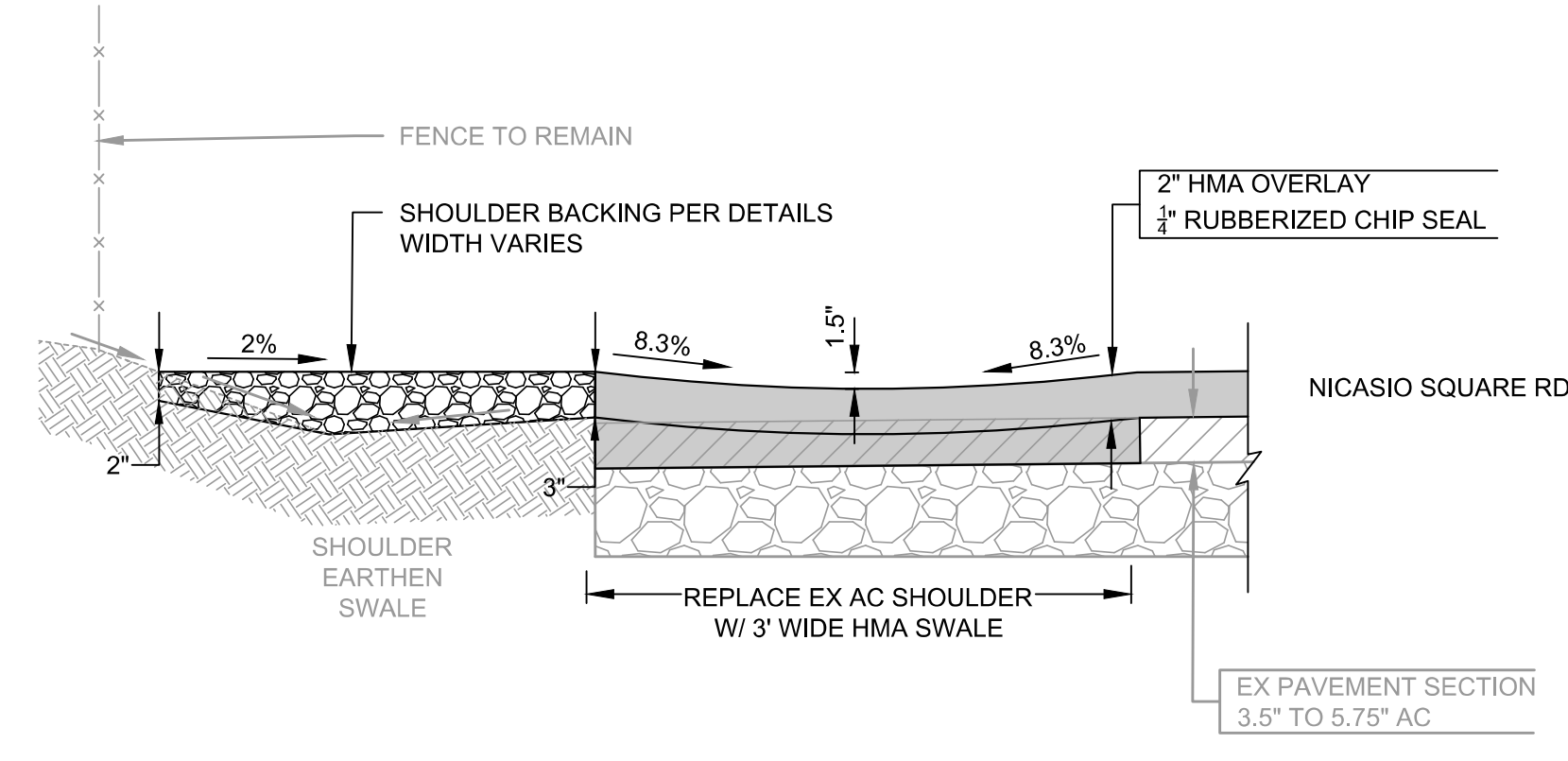
DIGOUT IN HMA MILL & FILL LOCATIONS WITHOUT FABRIC

**NOTES:**

- ENGINEER SHALL MARK DIGOUT LOCATIONS IN FIELD AND VERIFY QUANTITIES WITH CONTRACTOR PRIOR TO DIGOUT CONSTRUCTION
- AS APPLICABLE, UTILIZE GEOTEXTILE FABRIC AT BOTTOM OF DIGOUT WHERE SUBGRADE IS EXPOSED.

**4 TYPICAL DIGOUT DETAILS**

SCALE: N.T.S

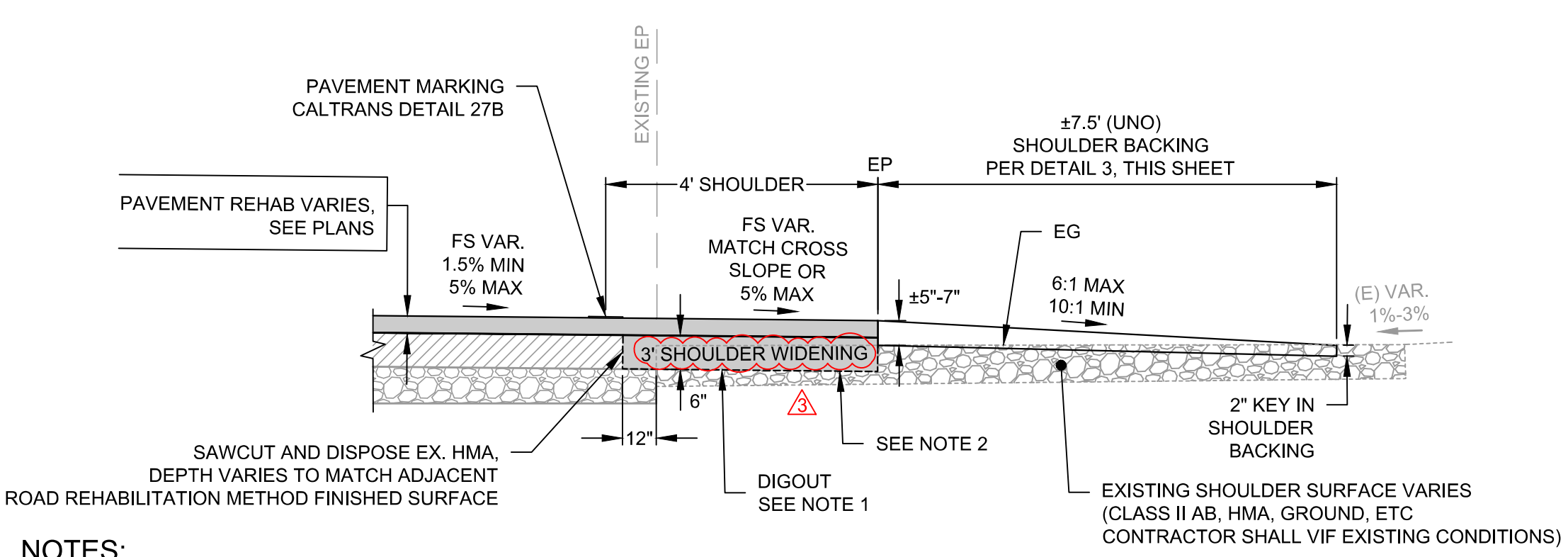


**NOTES:**

- MILL 3' WIDE ROADWAY SHOULDER AS INDICATED IN PLANS AND APPROVED BY THE ENGINEER.
- UTILIZE GEOTEXTILE FABRIC AT BOTTOM OF HMA WHERE SUBGRADE IS EXPOSED. CONTRACTOR TO TRACK ADDITIONAL TIME AND MATERIALS FOR INSTALLATION OF FABRIC FOR A CONTRACTOR CHANGE ORDER.
- AT CONFORM TO CHURCH PATHWAY, REDUCE SWALE SLOPES TO 5% MAX.

**5 HMA SWALE DETAIL**

SCALE: N.T.S

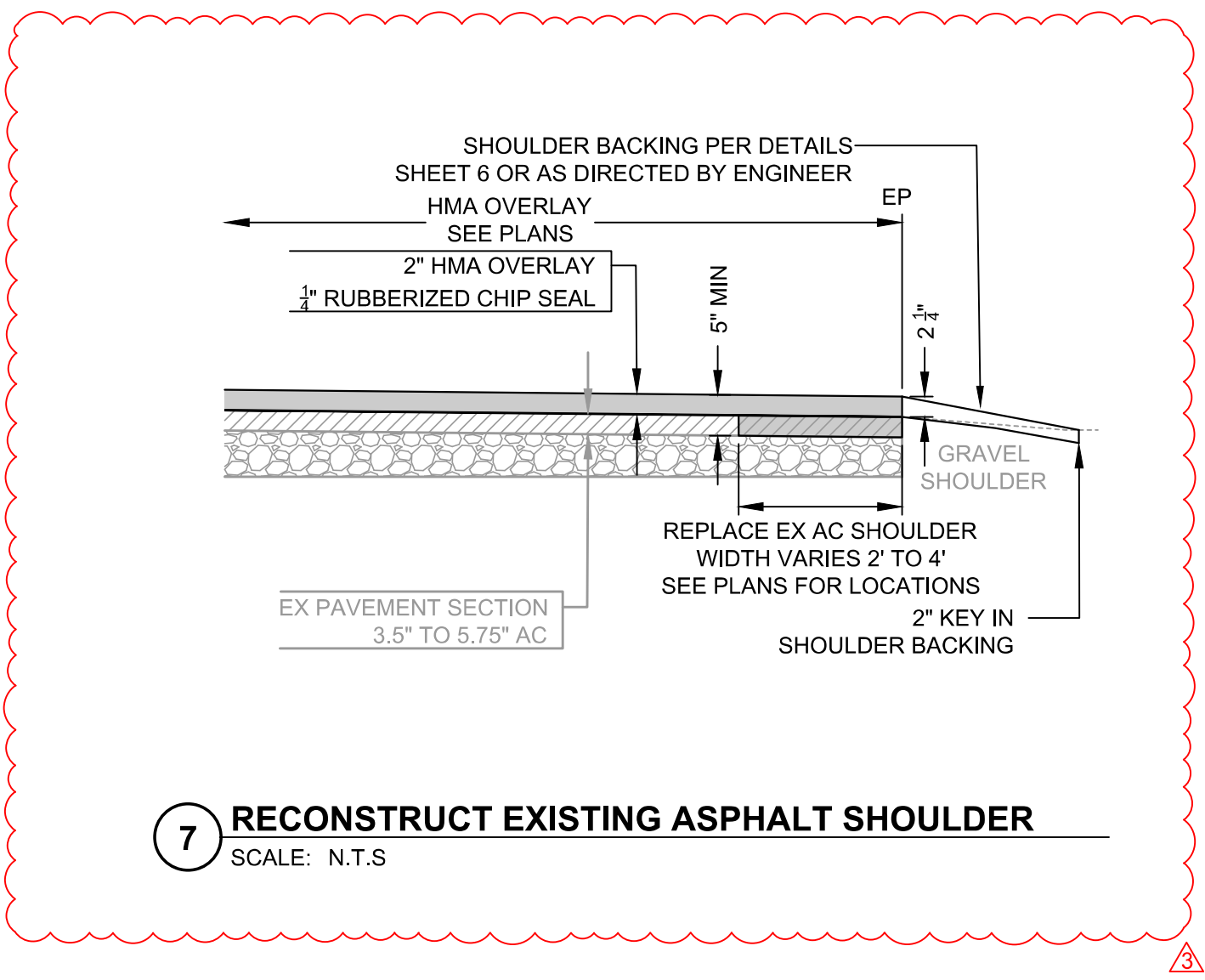


**NOTES:**

- CONTRACTOR SHALL PAVE FINAL LIFT WITH ADJACENT ROAD REHABILITATION FINAL LIFT.
- AS APPLICABLE, UTILIZE GEOTEXTILE FABRIC AT BOTTOM OF DIGOUT WHERE SUBGRADE IS EXPOSED.

**6 HMA SHOULDER DETAIL**

SCALE: N.T.S



**7 RECONSTRUCT EXISTING ASPHALT SHOULDER**

SCALE: N.T.S

Rev	Date	Description	Designed	Drawn	Checked
1	6/1/2026	ADDENDUM 3	RJS	RJS	RS
2	5/22/2026	ADDENDUM 1	RJS	RJS	RS
3	5/12/2026	BID SET	RJS	RJS	RS

NICASIO VALLEY ROAD PAVING PROJECT  
**DETAIL SHEET**  
3501 CIVIC CENTER DRIVE, SAN RAFAEL, CA 94903

Town of  
**Nicasio**  
County Of  
**Marin**  
State Of  
**California**

Prepared Under the Direction of:  
  
Richard J. Souza  
No. 67892  
CIVIL

Sheet  
**6**  
Scale: as shown  
Date: May 12, 2026  
County Plan No. 41EP2601D  
Sheet No. 6 of 6